



City of Harrisonburg, Virginia Planning Commission Meeting

June 11, 2014

7:00 p.m.

Regular Meeting
409 South Main Street

- 1) **Call to order, roll call, determination of quorum, and review/approval of minutes from the May 14, 2014 regular meeting.**
- 2) **New Business**

Special Use Permit – 1214 Windsor Road

Public hearing to consider a request from Erica Lynn Dorsey for a special use permit per Section 10-3-34 (6) of the Zoning Ordinance to allow a Major Family Day Home within the R-1, Single Family Residential District. Major family day homes can have from six to 12 children under the age of 13, exclusive of any children who reside in the home. The 21,444 +/- sq. ft. property is located at 1214 Windsor Road and is identified as tax map parcel 84-E-4.

Special Use Permit – 2477 Reservoir Street (Multi-Family Units – Campus View Apartments)

Public hearing to consider a request from Carmel Quinn Falls, Leon Nelson, and HGC, Inc. with representative Ed Blackwell of Blackwell Engineering for a special use permit per Section 10-3-48.4 (6) of the Zoning Ordinance to allow multiple family dwellings of up to 12 units per building within the R-3, Medium Density Residential District. The 2.039 +/- acre property is addressed as 2475 & 2477 Reservoir Street and identified as tax map parcel 81-A-8.

Zoning Ordinance Amendment – Junk Yards by SUP in M-1 (10-3-96, 97, & 99)

Public hearing to consider a request from Gerdau with representative Richard Gallegos to amend the Zoning Ordinance to allow junk yards within the M-1, General Industrial District by special use permit. The amendment would delete existing text within Section 10-3-96 (8) & (20), add junk yards as an available special use within Section 10-3-97, and add additional text within Section 10-3-99 (c).

Rezoning – 130 Mt. Clinton Pike M-1 to B-2C (Lantz Eby Enterprises)

Public hearing to consider a request from Lantz Eby Enterprises with representative Keith May of Cottonwood Commercial to rezone one parcel containing 3.0 +/- acres from M-1, General Industrial District to B-2C, General Business District Conditional. The property is located at the corner of Mt. Clinton Pike and Acorn Drive, addressed as 130 Mt. Clinton Pike and 302 Acorn Drive, and identified as tax map parcel 44-C-1.

Rezoning – Acorn, LC 2.989 Acres from M-1 to B-2C

Public hearing to consider a request from Acorn, LC with representative Ken Kline of Cottonwood Commercial to rezone 2.989 acres of a 94 acre tract (part of which is in Rockingham County) from M-1, General Industrial District to B-2C, General Business District Conditional. The parent tract has public street frontage along North Liberty Street, Acorn Drive and North Liberty Street. The subject acreage to be rezoned is located along the property's 364 feet of Mt. Clinton Pike street frontage. The property is identified as tax map parcel 44-C-2.

Staff will be available Monday July 7, 2014 at 4:30 p.m. for those interested in going on a field trip to view the sites for the July 9, 2014 agenda.

Preliminary Plat – Northside Heights (Northside, LLC)

Consider a request to from Northside, LLC and Joseph and Linda Moore to preliminarily subdivide 17 parcels and portions of Wilson Avenue and Boulevard Avenue, containing approximately 10.65 +/- acres, to create 8 parcels. The plat also includes dedicating public street right-of-way (60 ft. wide by 248 ft. long) for an extension of Technology Drive from the southern side of Mt. Clinton Pike. In addition, the plat is dedicating other public street right-of-way along the southern side Mt. Clinton Pike. All parcels are zoned R-2, Residential District. The properties are addressed as 1371, 1391, & 1411 North Main Street, 36, 44, 75, & 81 Wilson Avenue, and 25, 35, 45, 55, 65, & 75 Mt. Clinton Pike. The properties are identified as tax map parcels 42-B-6A, 7, 7A, 8, 8A, 8B, 8C, 9, 9A, 32, 33, 34, 35, 35A, & 36 and 44-A-30 & 31.

Rezoning – West of N. Main St. and South of Mt. Clinton Pike (R-2 to B-2C)

Public hearing to consider a request from Northside, LLC and Joseph and Linda Moore with representative Balzer & Associates, Inc. to rezone 6.69 +/- acres of property from R-2, Residential District to B-2C, General Business District Conditional. The subject property is made up from 7 parcels and portions of 6 parcels and portions of the existing public street right-of-way of Wilson Avenue and Boulevard Avenue. The applicants are in the process to close and purchase the necessary portions of Wilson and Boulevard Avenues. The properties are addressed as 1411 North Main Street, 36, 44, 75, & 81 Wilson Avenue, and 25, 35, 45, 55, & 65 Mt. Clinton Pike. The properties are identified as tax map parcels 42-B-8B, 8C, 32, 33, 34, 35, & 35A, portions of tax map parcels 42-B-8, 8A, 9, 9A, & 36 and 44-A-31, along with portions of Wilson Avenue and Boulevard Avenue found on tax map sheets 42 and 44.

Downtown Streetscape Plan

Public hearing to consider adopting the City of Harrisonburg Downtown Streetscape Plan. The plan's goal is to present an easily communicable, comprehensive vision for the public streetscape in Downtown Harrisonburg that can be utilized by public and private agencies to further develop and sustain a vibrant downtown.

- 3) Unfinished Business**
- 4) Public Input**
- 5) Report of secretary and committees**
Proactive Zoning
- 6) Other Matters**
- 7) Adjournment**

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City of Harrisonburg

Department of Planning and Community Development

409 South Main Street
Harrisonburg, Virginia 22801
540-432-7700

www.harrisonburgva.gov/community-development

Memorandum

To: Harrisonburg Planning Commission
From: Alison Banks, Senior Planner
RE: June 11, 2014 Regular Meeting
Date: Friday, June 6, 2014

There are eight items of New Business on the agenda for next week; however, two rezoning requests were withdrawn prior to finalizing the packet. The remaining six items include two special use permit requests, a zoning ordinance amendment, a preliminary plat, a rezoning request, and the Downtown Streetscape Plan.

The special use permit requests include a request for a major family day home at 1214 Windsor Road and a request for multi-family units (apartments) along Reservoir Street – this is an extension of the existing Campus View. The zoning ordinance amendment is a request from Gerdau (a leading steel producer in the Americas with a recycling division) to add “junk yards” as a special use within the M-1, General Industrial District. Staff is recommending in favor of these requests.

The preliminary plat and rezoning are from the same parties that requested to close portions of Wilson Avenue and Boulevard Avenue last month. As explained last month, the preliminary plat and rezoning request are part of a multi-step process the applicants are initiating so they can enter into contract with an interested buyer to construct what they hope is commercial development. Staff is offering a favorable recommendation for both requests.

Also on the agenda, is the public hearing for the Downtown Streetscape Plan – a drop box link was provided to you previously and hopefully you were able to download the document and begin to review it. If not it is enclosed within!

We will tour the sites on Monday at 4:30 p.m. **Please let us know in advance if you will not be attending the tour.** See you next week.



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Preliminary Plat – Northside Heights (Northside, LLC)

Consider a request to from Northside, LLC and Joseph and Linda Moore to preliminarily subdivide 17 parcels and portions of Wilson Avenue and Boulevard Avenue, containing approximately 10.65 +/- acres, to create 8 parcels. The plat also includes dedicating public street right-of-way (60 ft. wide by 248 ft. long) for an extension of Technology Drive from the southern side of Mt. Clinton Pike. In addition, the plat is dedicating other public street right-of-way along the southern side Mt. Clinton Pike. All parcels are zoned R-2, Residential District. The properties are addressed as 1371, 1391, & 1411 North Main Street, 36, 44, 75, & 81 Wilson Avenue, and 25, 35, 45, 55, 65, & 75 Mt. Clinton Pike. The properties are identified as tax map parcels 42-B-6A, 7, 7A, 8, 8A, 8B, 8C, 9, 9A, 32, 33, 34, 35, 35A, & 36 and 44-A-30 & 31.

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MINUTES OF HARRISONBURG PLANNING COMMISSION
May 14, 2014

The Harrisonburg Planning Commission held its regular meeting on Wednesday, May 14, 2014 at 7:00 p.m. in the City Council Chambers, 409 South Main Street.

Members present: Richard Baugh, Gil Colman, MuAwia Da'Mes, Judith Dilts, Deb Fitzgerald, and Jefferson Heatwole.

Members absent: Henry Way

Also present: Stacy Turner, Director of Planning and Community Development; Adam Fletcher, City Planner; Alison Banks, Senior Planner and Secretary.

Chair Fitzgerald called the meeting to order and determined there was a quorum with six members in attendance. She then asked if there were any corrections, comments or a motion regarding the minutes from the April 9, 2014 Planning Commission meeting.

Mrs. Banks noted the change in the minutes regarding the word "tenant" to "tenet."

Mr. Da'Mes moved to approve the minutes as presented from the April 9, 2014 regular Planning Commission meeting.

Dr. Dilts seconded the motion.

All members voted in favor of approving the April 2014 minutes.

New Business

Street Closing – Wilson Avenue and Boulevard Avenue

Chair Fitzgerald read the request and asked staff to review.

Mr. Fletcher said the following land uses are located on and adjacent to the property:

Site: Wilson Avenue is a substandard public street (a portion of which is a paper street) that intersects North Main Street approximately 300 feet south of the North Main Street/Mt. Clinton Pike intersection and runs parallel to Mt. Clinton Pike for approximately 1,320 feet. Boulevard Avenue is an undeveloped public street located off of Wilson Avenue extending about 690 feet to the south.

North: Undeveloped property, zoned R-2

East: Single family homes fronting Wilson Avenue and North Main Street, zoned R-2

South: Single family homes fronting Wilson Avenue and North Main Street, zoned R-2; a non-conforming single family home fronting North Main Street, zoned B-2; the Harrisonburg Rockingham Community Services Board property fronting North main Street, zoned B-2; and property owned by GSW Investors, zoned M-1

West: GSW Investors property, including operations of Rockingham Construction and Special Fleet Service, Inc., zoned M-1

Northside, LLC (the applicant), a family owned LLC, is requesting to close 62,004 +/- square feet of public street right-of-way (ROW) of two separate streets: Wilson Avenue and Boulevard Avenue. The closure request is the first step of a multi-step process the applicant is initiating so they can enter into contract with an interested buyer to construct what they hope is commercial

development—conceptually, a grocery store. After this month's review of the ROW closing, a preliminary plat and rezoning request are planned to be simultaneously reviewed next month. The reason the closure request is occurring first, is because if the City is not interested in closing the ROW, then the interested buyer does not want to move forward as their conceptual building layout utilizes areas that are now public street ROW.

There are multiple layouts and maps within the packet to assist in understanding this request. The applicant has provided three different layout sheets. Sheet EX-1 illustrates the public ROW requested for closure. Sheet EX-2 demonstrates a preliminary layout of what their parcels could look like after the ROW is incorporated into their adjoining properties along with showing a planned public street ROW dedication, which essentially is an extension of Technology Drive. This same sheet also roughly depicts what the preliminary plat will look like for next month's Planning Commission meeting. Sheet EX-3 illustrates the portion of property they plan to request be rezoned to B-2 and shows a different configuration of what the surrounding parcels could look like if the area is rezoned.

The applicant owns 25 parcels adding up to about 1.66 acres of property in this area of the City with public street frontage along Wilson Avenue, Boulevard Avenue, Mt. Clinton Pike, and North Main Street. About 4.14 acres (four parcels) are located on the northern side of Mt. Clinton Pike at the corner of Mt. Clinton Pike and North Main Street. This property is zoned M-1 and is home to Harrisonburg Motor Express, a trucking company owned by a partner of Northside, LLC. At this time, that property has been unassociated with the development plans that have been discussed with staff. The majority of their property (2.52 acres) and the larger area desired for commercial development is located on the southern side of Mt. Clinton Pike and the western side of North Main Street. Aside from the corner parcel—a 30,831 square foot lot zoned M-1—all other property is zoned R-2. In addition to this acreage, Joseph and Linda Moore (Joseph being the owner of Harrisonburg Motor Express), own 1.99 acres made up of two parcels zoned R-2; they reside at that site. In all, 14.51 acres is the total area the applicant and Joseph and Linda Moore together are hoping to sell for development. (Staff has provided a map within the packet depicting the information just described.)

With regard to the streets requested for closure, Wilson Avenue is a substandard public street (a portion of which is a paper street) that intersects North Main Street approximately 300 feet south of the North Main Street/Mt. Clinton Pike intersection. The entire public street ROW runs parallel to Mt. Clinton Pike for about 1,320 feet, where roughly 670 feet is paved. Over half of the length of ROW is 50 feet wide, while the remaining portion is 30 feet wide. Boulevard Avenue is an undeveloped paper street located off of Wilson Avenue extending about 690 feet to the south. Boulevard Avenue is 30 feet wide. The paved section of Wilson Avenue does not extend to its intersection with Boulevard Avenue, and it currently only serves four single family detached structures, three of which are owned by the applicant and the fourth being owned by Joseph and Linda Moore.

The applicant is requesting to close Wilson Avenue from North Main Street to just beyond its intersection with Boulevard Avenue—about 835 feet in length, most of which is 50 feet wide—while requesting to close Boulevard Avenue in its entirety. If approved as requested, there would remain approximately 485 feet in length of Wilson Avenue as undeveloped public street ROW. As planned, this remaining ROW will not be landlocked from other public street ROW because, as shown on Sheet EX-2, the planned extension of Technology Drive would intersect this undeveloped remaining portion of Wilson Avenue.

As is standard practice, all property owners adjacent to the requested areas for closure will have the opportunity to purchase up to 50 percent of the ROW width along the entire length adjoining their property. In all, 16 parcels are adjacent to the ROW requested for closure; nine are owned by the applicant, two are owned by Joseph and Linda Moore, two are owned by GSW Investors, one is owned by Harrisonburg Rockingham Community Services Board, one is owned by Harrisonburg Rockingham Community Mental Health and Others, and one is owned by Richard and Betty Sampson.

As shown on Sheet EX-1, there is City public water and sewer infrastructure as well as overhead utility lines owned by HEC within and adjacent to the areas requested for closure. If approved, the City will reserve easements for all of these utilities in the existing ROW with the ordinance that dedicates the property to the new owners. The applicant is aware of this issue and is preliminarily showing on Sheet EX-1 the locations of where easements should be established.

The applicant must understand that if the ROW is closed and purchased, the City will no longer provide snow removal or any other maintenance services on Wilson Avenue. With regard to sanitation services (trash pick-up, recycling, etc.), unless special arrangements are worked out with the Department of Public Works to continue traveling the paved section of Wilson Avenue, residents will have to place their sanitation items along the street their parcel ends up having street frontage upon. Public school bus services would be provided as determined necessary.

Aside from the matters described here, the City does not need to maintain ownership of the subject ROWs to provide any other City services. Staff recommends closing the 62,004 +/- square feet of ROW of Wilson Avenue and Boulevard Avenue only with the following two conditions:

1. The City shall reserve easements for all public utilities in the subject areas.
2. The recording of the street closing shall not occur until, or simultaneously with, the dedication of the extension of Technology Drive.

It should be understood that the City cannot accept the dedication of the extension of Technology Drive until the associated preliminary plat is approved by Planning Commission. The final plat that dedicates Technology Drive (which only requires administrative review), cannot be approved until the completion of the project is built or a form of surety is accepted by the City to cover all public improvements.

It should be further understood that staff's favorable recommendation for the ROW closure request provides no bearing upon any future development proposal—including the planned preliminary plat and rezoning.

Chair Fitzgerald asked if there were any questions for staff.

Mr. Colman asked if there would be any future consideration given to closing the rest of Wilson Avenue.

Mr. Fletcher said the original proposal requested all of Wilson Avenue be closed; however the applicant did not need the entire length of the street. After discussion between City staff and the applicant it was agreed that it made most sense to keep it open at this point so that if the adjacent properties ever developed, they could access and have a public street built off of that internal public street as opposed to multiple entrances along Mt. Clinton Pike. Of course that will require more dedication of public right-of-way to build that street because it is insufficient right-of-way at this time.

Mr. Heatwole said am I to understand that the portion of Wilson Avenue that would not be closed would eventually connect to Technology Drive right-of-way extended in this area?

Mr. Fletcher replied yes.

Mr. Da'Mes asked if all the adjacent property owners had been notified, or talked to, in regards to what the process is for obtaining that property.

Mr. Fletcher said to my understanding the applicants have spoken to everyone except Richard and Betty Sampson, they were unable to connect with them; but the applicants can probably further discuss that with you. A very large property owner adjacent to this is the GW Investors property, and I met with them directly just to get their understanding and to find out where they fit into this plan. They do not have a position one way or the other; as long as they have access out to Mt. Clinton Pike if Boulevard Avenue gets closed.

But to answer your questions, tonight's procedure is not a public hearing; therefore, the adjacent property owners were not notified. It is a public hearing at City Council next month and all adjoining property owners will be notified.

Mr. Colman asked if the applicant's intent was to purchase all of Boulevard Avenue.

Mr. Fletcher said they do not need it; whether or not they are interested in buying it is up to them.

Mr. Colman said does that leave a potential for a "no man's land" situation if the City closes the street and the applicant's do not purchase it?

Mr. Fletcher said the adjacent property owners would get first right of refusal on the right-of-way and if they do not want it the applicants could acquire it. You would have to ask them if they were interested in it; but the City would not just leave it open, someone would have to buy it.

Chair Fitzgerald asked if there were any further questions for staff. Hearing none, she said this is not a public hearing; however we do invite the applicant or the applicant's representative to come forward and speak.

Mr. Bill Moore with Balze and Associates, the consulting engineer for the applicant and developer, said we can answer any questions you may have regarding the street closures. We do want to clarify one thing on the final plan - posting a bond for Technology Drive would allow the final plat to be approved, correct.

Mr. Fletcher replied yes.

Chair Fitzgerald asked if there were any questions for the applicant's representative. Hearing none, she asked if there was any further discussion.

Mr. John Serrell, 907 Fairway Drive, said he owns property at the intersection of Technology Drive and Mt. Clinton Pike, and he is in favor of this project.

Chair Fitzgerald asked if there was anything further or perhaps a motion.

Mr. Baugh said on the second recommended condition are we intending to say that the recording of the street closing could be simultaneous with the street dedication, just not before?

Mr. Fletcher replied that is correct.

Mr. Baugh said that is what I thought it meant, but when I read it I am not sure that it says that. Could we not just say “the recording of the street closing shall not occur before the dedication of the extension of Technology Drive?”

Mr. Fletcher said that is fine, that works.

Mrs. Turner said I have a question. Should we have included a condition that the roadway would not be closed unless the preliminary plat, showing that all lots will have street frontages as required, be recorded at the same time? Is there another mechanism by which that is assured to happen?

Mr. Fletcher said when I was looking at this it was with the assumption that the preliminary plat and the dedication of the street are all the same plat. But it is a good point because we do not have a plat at this time; therefore, I think it is a good condition to have. There should be a condition that the platting of the lots shall occur in the same way as the extension of Technology Drive. Perhaps it could be just as simple as a condition ensuring that all newly created lots have public street frontage.

Dr. Dilts moved to recommend approval of the street closings with the three conditions:

- the City shall reserve easements for all public utilities in the subject areas;
- the recording of the street closing shall not occur before the dedication of the extension of Technology Drive; and
- all newly created lots have public street frontage.

Mr. Colman seconded the motion.

Chair Fitzgerald called for a voice vote on the motion.

All voted in favor of the motion (5-0) to recommend approval of the street closings with the three conditions.

Chair Fitzgerald said this item will be before City Council on June 10th.

Zoning Ordinance Amendment – Article Y. Floodplain Zoning District (2014 Amendment)

Chair Fitzgerald read the request and asked staff to review.

Mrs. Fanks said The City of Harrisonburg is requesting adoption of an updated Article Y, Floodplain Zoning District of the City’s Zoning Ordinance per requirements of the Federal Emergency Management Agency (FEMA).

The Virginia Department of Conservation & Recreation (DCR), on behalf of FEMA, periodically conducts Community Assistance Visits (CAVs) to Virginia communities that participate in the National Flood Insurance Program (NFIP). The primary purpose of the CAV is to help the City maintain compliance with NFIP requirements. City Staff met with a representative of DCR as part of a recent CAV and the outcome of the meeting identified some changes that needed to be made within Article Y in order to maintain compliance with the NFIP requirements. After evaluating the needed changes, rather than trying to modify the existing text, staff chose to adapt FEMA’s 2011 Virginia Model Floodplain Ordinance text specific to the City’s needs, thus ensuring all current requirements would be met. This update is very similar to the Floodplain Zoning District amendment City Council approved in 2007; however at that time the flood map was revised as well. A formal adoption of a compliant floodplain ordinance must be completed this summer to complete the CAV review.

Staff recommends adopting the updated Article Y, Floodplain Zoning District of the Zoning Ordinance.

Chair Fitzgerald asked if there were any questions for staff at this time.

Mr. Colman said I have a couple of questions and one of them is with the definitions section. With the current ordinance, the definitions are listed in the front of the ordinance; it is always useful to have the definitions at the beginning so you know what you are reading when you are going through the ordinance. The proposed amendment has the definitions in the back; I do not know if this is something we can change or not.

Mrs. Banks said if it is a preference of the Planning Commission we can certainly change that.

Mr. Fletcher said the existing Floodplain Ordinance from 2008 was also the model so the model changed as well.

Mr. Colman continued saying in the current regulations it lists penalties and but they are not listed in the new regulations.

Mrs. Banks said the Floodplain regulations are part of the Zoning Ordinance and therefore we refer back to penalties within the zoning regulations for the floodplain.

Mr. Colman said in terms of Section 10-3-163, Use and Interpretation of FIRMS, in my experience the map is what determined if a property was in the floodplain or not. This section is basically saying that if something seems to be in the floodplain or if the Floodplain Administrator makes the determination to have a survey done, then that property could possibly become part of the Floodplain. This is not something we have done before and I feel it is significant.

Mr. Fletcher said this is one of the sections that we proposed to DCR that we would like to remove and they wrote back and said this section needs to remain in the regulations and should not be deleted.

Mrs. Turner said I believe that you are remembering; but you are correct, I do not specifically recall about Section 10-3-163.

Mrs. Banks said Section 10-3-163 is a new section to the regulations.

Staff discussed with Planning Commission the specifics of Section 10-3-162 (17) which states "it is the duty of the Community Floodplain Administrator to take into account flood, mudslide and flood-related erosion hazards, to the extent that they are known, in all official actions relating to land management and use throughout the entire jurisdictional area of the Community, whether or not those hazards have been specifically delineated geographically (e.g. via mapping or surveying)." FEMA has explained that this should be included within the regulations to allow more flexibility for the Floodplain Administrator.

Chair Fitzgerald asked how much flexibility does the Floodplain Administrator need.

Mrs. Turner replied that FEMA cannot answer that question; honestly there are some things within the ordinance that FEMA can just not clearly answer for us.

Mr. Colman said I can see where there are some situations when you look at the profile and the floodplain should be further out than it is based on the topography. Do you look at the profile or the actual map?

Mrs. Turner said that is exactly one of those things that just does not make a whole lot of sense when interpreting the regulations. We just have to have it there. This is one reason why we decided to go with the model; FEMA wanted these items within the regulations.

There was one item that we could remove, page 15, under definitions; FEMA did say we could remove this item. It is the definition of Coastal A Zone, we can remove it from our regulations; we do not have it.

Another definitions item that is a change is under substantial improvement; if you are new construction or substantial improvement then certain things apply to you. Currently, the definition of substantial improvement says the term does not however include any alteration of a historic structure; therefore the way our ordinance is currently worded historic structures are exempt from the regulations. We work with people who own historic structures and try to get them to comply to the best that we can when they are renovating. The new regulations change that and basically say that they have to comply up until the point where it would preclude their designation as a historic structure or a contributing structure to a historic district. The new ordinance states that "documentation that a specific ordinance requirement will cause removal of the structure from the National Register of Historic Places or the State Inventory of Historic places must be obtained from the Secretary of the Interior or the State Historic Preservation officer."

Mr. Heatwole asked if there was any way to modify these regulations.

Mrs. Turner replied if we want to continue to be part of the NFD then we need to adopt these requirements.

Chair Fitzgerald asked if there were further questions for staff. Hearing none, she opened the public hearing and asked if there was anyone wishing to speak.

Mr. John Serrell, 109 Fairway Drive, said he owns property that, when purchased in 2002, it was not in the floodplain. A motel was developed next to my property on a site on which nothing existed for nine years. They raised the property over five feet and now the motel is out of the floodway and my property is in it. I have engineering data to Stacy over a year ago and asked her to review it for my engineers, and it was never reviewed. I wrote letters several months ago to Community Development and asked for specific engineering and legal questions to be answered; they have never been answered. I met with the City Attorney and he promised me answers; I have not got them. Mayor Ted Byrd has promised me in writing answers to my questions and I have not gotten any answers.

I filled my property and now it is two feet higher than the motel. The motel is certified as one foot out of the floodway, so how can my property still be considered in the floodway and the motel not? I have asked for answers to these questions, I have been promised answers and I have not received any. I would like to see all of this (Floodplain Ordinance Amendment) tabled until all my questions are answered. Please come out to my property and see what I am talking about. I have been ignored by the City and this is the perfect time to tell you all to come to my site and see what I am talking about.

I am paying \$10,000 a year and lost a sale of my property because of this; I do not care about that, it is not an issue. But, when I am not treated fairly I do not appreciate it.

Thank you for your time.

Chair Fitzgerald asked if there was anyone else wishing to speak. Seeing none, she closed the public hearing. She then asked staff if they would like to address the speaker's comments.

Mr. Fletcher said the changes to the regulations proposed tonight do not change the floodplain map.

Mrs. Turner said as far as what he has asked, we have discussed it with FEMA and they found there was not a problem. We take our direction from FEMA with regard to floodplain matters. Mr. Serrell has been answered multiple times, maybe not this last time he asked; but he was answered the three or four times before when he asked similar questions. The last I heard of this was that the City Attorney was going to contact him about it. I do not know if that happened, I can check into that. I do not know what answer he can get from the City Attorney at this point, FEMA has provided his answer.

Mr. Colman asked if his property was in the floodplain or the floodway as he referred to it.

Mrs. Turner said it is in the floodplain, I do not believe he has anything in the floodway.

Mr. Colman asked if the property was in the floodplain in the old maps (prior to the 2008 change).

Mrs. Turner replied no.

Mr. Colman said he filled his property so he can be identified out of the floodplain.

Mrs. Turner said he would still need to apply for a letter of map revision (LOMR) and he is not interested in pursuing that level of investment into it.

Mr. Colman said so there is a course of action and if he does not want to follow the course of action that is on him.

Mr. Baugh said I cannot comment on what discussions he has had with the Mayor or the City Attorney, but I can generally say this has been going on for years with Mr. Serrell, it is not new information. He is somewhat insistent that the solution be handled a certain way.

Mr. Heatwole questioned how the Serrell property could be one foot higher than the motel, yet still be in the floodplain.

Mr. Fletcher said what he has done on his property may make the site higher, but for it to be recognized on the map he first has to apply for a LOMR so that FEMA recognizes the property as out of the floodplain. Mr. Serrell does not want to do that.

Mr. DeMes said he is paying extra flood insurance each year, which is quite significant. There has to be more to this than what we are hearing.

Mr. Heatwole asked if the City had an idea of how much a LOMR would cost.

Mr. Fletcher replied no.

Mr. Colman said you would have to pay an engineer to do the study and then provide the calculations to FEMA. Then turn in the application to the City and the City would apply to FEMA. That is how the process works. I do not see any other way around it for this site; the City cannot decide if a property is no longer in the floodplain.

Chair Fitzgerald asked if there were any further questions or discussion regarding the amendment.

Mr. Colman moved to recommend approval of the proposed Floodplain Ordinance Amendment, Article Y, Floodplain Zoning District with the discussed changes to the formatting and the removal of the Costal A Zone definition.

Dr. Dilts seconded the motion.

Chair Fitzgerald called for a voice vote on the motion. All voted in favor (6-0) of the motion to recommend approval.

Chair Fitzgerald said this will be heard at the June 10th City Council meeting.

Unfinished Business

None.

Public Input

Misty Cook of A+ Barber & Styling Salon, LLC on Erickson Avenue said she was here this evening with several of her co-workers/investors to speak about signs. We opened our business in February. These ladies have been stylists in Harrisonburg and have held City Business Licenses for more than fifteen years and have finally ventured out on their own. As with any small business, when you go out on your own you have a very small budget; because of this we purchased flying banner signs (feather signs). You see these types of signs all over town. Apparently someone made a complaint about our signs and our landlord received a letter from the City informing us they need to come down. We were unaware that there was an ordinance that did not allow for this type of sign.

I did contact the City to find out more information and we did take the signs down. I did ask what the procedure was to get this regulation changed. We did a short drive around Harrisonburg after we received our letter and we found 27 businesses that have this type of sign. Many of these are small, local businesses relying on cheap advertising. We also found six electronic signs which range from small business to big business; but from my understanding these electronic signs are allowed. When I spoke with the City I was informed that the flying banner signs are distracting to drivers; my argument is there is a lot more distracting driving going on. Are these flashing neon, LED signs not just as distracting as this fluttering sign?

We would like to ask that the ordinance be looked at and changed. Just by driving around Harrisonburg we feel that many businesses are not even aware that this ordinance is even in effect. I found out from talking with City staff that even the little yard signs are illegal; we found so many of those at businesses that we did not even count the total number. We feel we have a valid argument. We feel if the banners are in good repair and if they are attractively done, there is nothing aesthetically wrong with them. Personally, they have been a very valuable tool for our business. This was an inexpensive way for us to put our name out there. The other problem we have is our business is actually in the rear of the building, so we are not seen from Erickson Avenue. It's really easy for our business to get walk-in customers when the signs are out. We have noticed that since we took the signs down we have had a significant decrease in our walk-in traffic; the only advertising we changed was to remove the signs.

Thank you for listening.

Chair Fitzgerald said I understand that City Council talked about this issue.

Mr. Baugh replied yes we did and I was planning to bring this up tonight. Sign regulations are one of these areas that do not have to come to Planning Commission, yet often they do; that was the sense of City Council on this issue. Council is not saying they want a change to allow this; and they are not saying they want the sign regulations to remain unchanged. They more or less passed this along because we have a constituent with an issue and they are struggling with whom do I actually

need to talk to in order to get something started. Council generally referred it to Planning Commission to do with it as Planning Commission feels like it wants or wants not to.

I know the Mayor has made a couple of comments about this issue as part of the Comprehensive Plan review and while it could be done, we are probably just at the half way point before Comprehensive Plan review again; therefore, if we want to look at this in the Comprehensive Plan review that is about two years down the road. We have had some history with the sign ordinance and we have made some amendments when constituents came to us and said they just found out their sign was illegal. Then staff has said, given the things we are trying to accomplish this change might be appropriate at this time. So we do have a history of revising the regulations.

Mr. Da'Mes asked if Planning Commission could have a brief overview of the sign amendment that was done last night at City Council; this amendment was not vetted through Planning Commission.

Mrs. Turner said sign regulations are not part of the Zoning Ordinance; therefore, changing the sign regulations does not require a public hearing and it does not require a referral to or a recommendation from Planning Commission. Historically, staff has not taken sign amendments to Planning Commission first; we have only done so if for some reason City Council has asked us to.

Mr. Baugh said the amendment heard at Council last night was an issue that came up from a constituent and it went to staff for their input. The "punch line" of staff's report regarding the ordinance the City has in prohibiting a specific type of sign was that when staff called around to other jurisdictions and found we are the only place that has a prohibition on this type of signage, they recommended it be changed.

There is no inherent role of Planning Commission in this. It has been the case where Council has referred some of these matters to Planning Commission and if it is a minor tweak or something fairly straight forward it tends to just be done between staff and Council. Something like this seems to have potentially broader implications.

Mr. Da'Mes asked where are we in terms of things that are in our queue for review. I know we have telecommunication but what is after that?

Mr. Fletcher said things that are getting pushed forward are food trucks, community gardens and horticulture uses in residential districts.

Mrs. Turner said we do ordinance amendments on a continual basis, so it is almost every other month that we are doing an amendment and it is usually to address something someone has brought up. But like Adam was saying we do have a list of things that we would like to get taken care of.

Mr. Heatwole said since this has been referred by City Council to Planning Commission, let's at least put it in the queue.

Mrs. Turner said I would interpret that first staff would like to have some sort of indication from Planning Commission whether you like these types of signs enough to want to see them legalized in some form or fashion. If you are in favor of legalizing this type of signage, let us know and we will look into it and bring you some type of options. But we do not want to spend time doing this if there is a consensus among Planning Commission that you do not like these signs enough to do this.

Mr. Da'Mes said my feelings on this is I want to look at it from a holistic picture. I believe two years ago when we discussed feather signs I believe it was a relatively new concept for Harrisonburg. Now you have LED signs and reader boards, perhaps our ordinance is a little outdated to address those.

Mr. Baugh said I think an argument against these signs is that in a lot of places they have tended to not be well maintained signs. Then you raise the question of what do you do about the not well maintained sign. Remember, we enforce on a compliant basis. We do not have the staff to be proactive and run around checking on these violations; so changing the regulations is somewhat interrelated to hiring more staff.

Dr. Dilts said part of this is that this type of signage can appear “willy-nilly” anywhere on a property. Can you imagine a worse case scenario, if you take an entire block and every business has multiples of these signs? I think if we are going to consider this we need to look at the sign regulations as a whole and what do we want our City to look like in the end.

Mrs. Turner said I completely understand that logic; however if we are going to look at it that way we are putting this business’s immediate concern off for a substantial period of time. Even if staff was ready to give full attention to rewriting the sign regulations it will take multiple work sessions and possibly more than a year, before we would even have an attempt at a draft. Honestly, I believe it would take three or more work sessions just for you all to understand our sign regulations.

If we are not going to add staff to enforce regulations regarding this type of signage, then the best thing we could do would be to write an ordinance that hypothetically allows every business to have this type of flag sign in place three times a year for thirty days. This regulation would not keep people from putting them up illegally. We probably would not even have the staff to be going out after thirty days to make sure the sign came down or to look in to the illegally placed signs.

Staff is open to discussing a whole new sign ordinance but it is not going to resolve the immediate concern in this case.

Mr. Heatwole asked if there was a way to address the immediate concern.

Mrs. Turner replied: I have said the Planning Commission needs to decide how interested you are in allowing more opportunities for this type of sign to legally exist.

Dr. Dilts said if one of the reasons they are illegal is because they are considered a distraction, it would seem to me that we just do not want them.

Mrs. Turner said that is what the Planning Commission needs to discuss.

Mr. Fletcher said I believe there would be more people against this type of signage than for it. Staff will do whatever this body tells us to do.

Mr. Dames said I cannot speak for the group but I would feel generally not in favor of them. Having said that I am thinking maybe there is a time, like a new opening of an establishment, they would be appropriate.

Mr. Colman agreed and said there are certain times of the year that these signs might be appropriate.

After further discussion Planning Commission agreed that review of the sign regulations was something that should be put in the queue, not to trump those items already listed.

Mr. Fletcher informed the constituents that they could prepare something on their own and then go back to City Council with prepared language for an amendment.

Report of Secretary and Committees

Mrs. Banks said proactive zoning inspectors visited the Pleasant Hill area this month where they found nine violations consisting of inoperable vehicles. Next month inspectors will be in the Avalon Woods Area of the City.

Mr. Baugh said at City Council last night we took up and approved the variances and plat on the Village at Chicago Park.

Other Matters

Mr. Fletcher said next month will be a busy and exciting month. We have three rezonings, two special use permits, two preliminary plats, and one ordinance amendment. As of today the Downtown Streetscape Plan is on the agenda as well.

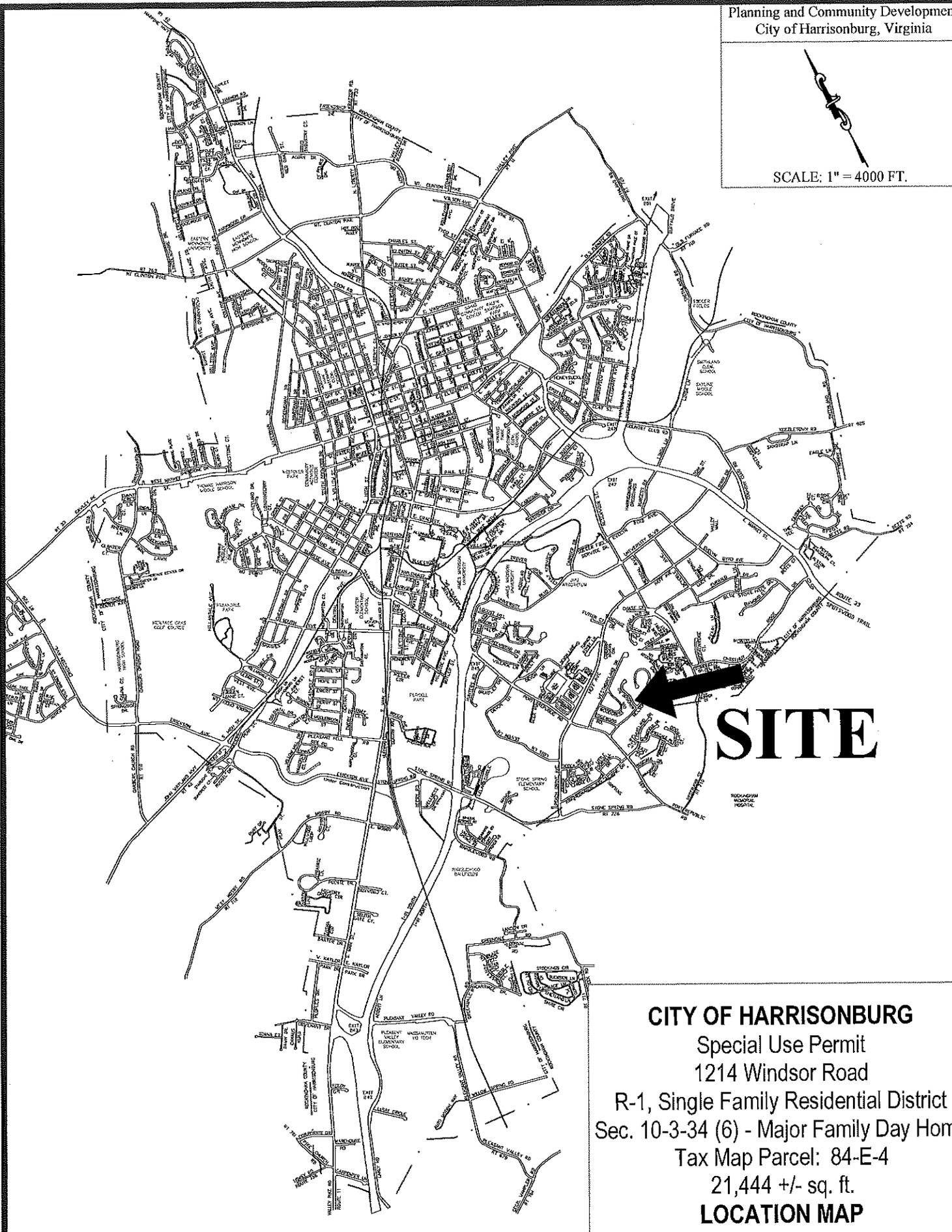
Adjournment

Planning Commission adjourned at 8:35 p.m.

DRAFT



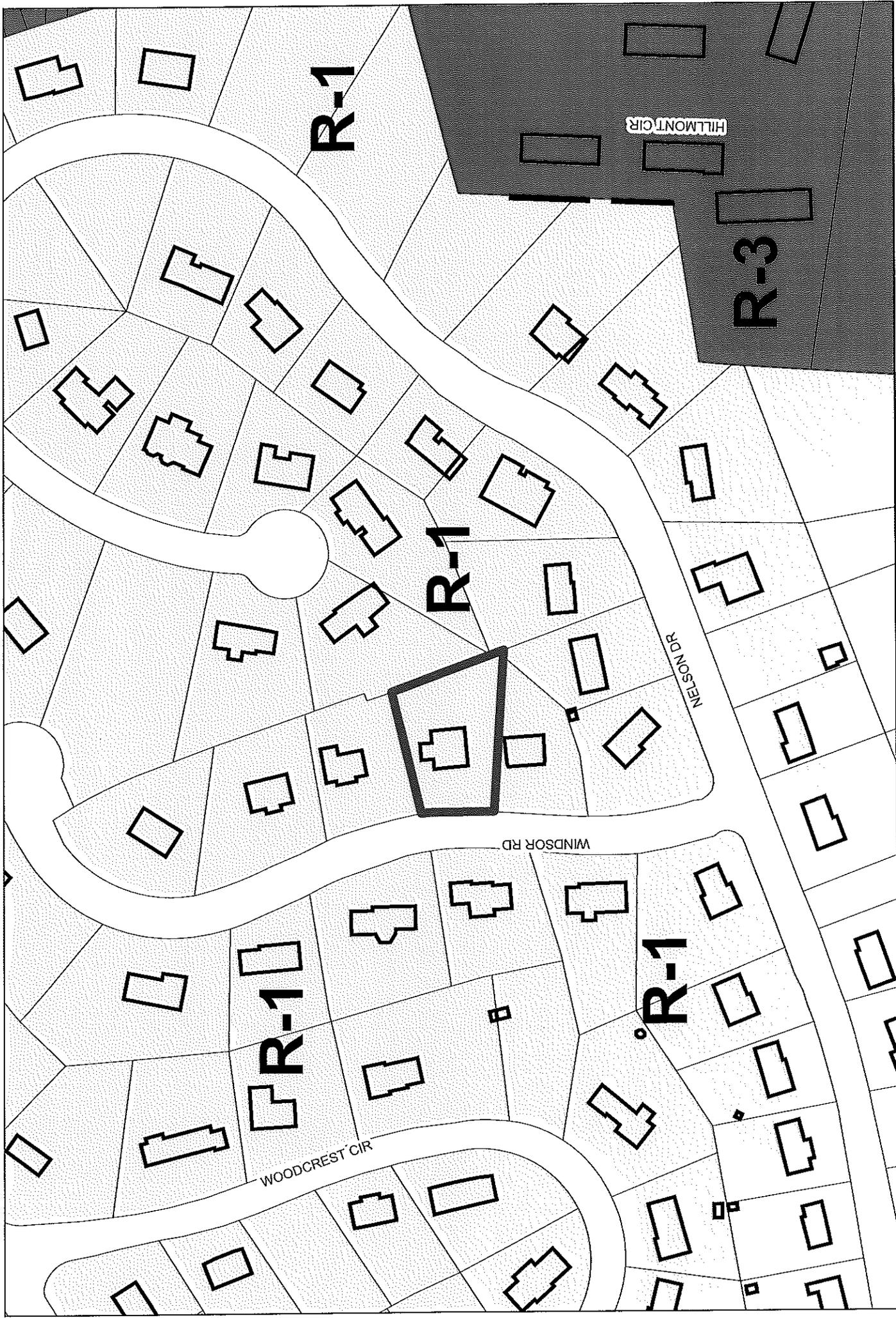
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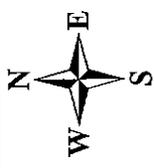
SITE

ROCKINGHAM
MEMORIAL
HOSPITAL

CITY OF HARRISONBURG
Special Use Permit
1214 Windsor Road
R-1, Single Family Residential District
Sec. 10-3-34 (6) - Major Family Day Home
Tax Map Parcel: 84-E-4
21,444 +/- sq. ft.
LOCATION MAP



**SUP - Major Family Day Home
1214 Windsor Road**





City of Harrisonburg, Virginia

DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT

STAFF REPORT

June 11, 2014

SPECIAL USE PERMIT-1214 WINDSOR ROAD-MAJOR FAMILY DAY HOME - 10-3-34(6)

GENERAL INFORMATION

Applicant: Erica Lynn Dorsey

Tax Map: 84-E-4

Acreage: 23,672 square feet

Location: 1214 Windsor Road

Request: Public hearing to consider a request for a special use permit per Section 10-3-34 (6) of the Zoning Ordinance to allow a major family day home within the R-1, Single Family Residential District.

LAND USE, ZONING, AND SITE CHARACTERISTICS

The Comprehensive Plan designates this area as Low Density Residential. This designation states that these areas consist of single-family detached dwellings with a maximum density of 1 to 4 units per acre. Low-density sections are found mainly in well-established neighborhoods and are designed to maintain the existing character of neighborhoods and to provide traditional areas for home ownership.

The following land uses are located on and adjacent to the property:

Site: Single-family dwelling and minor family day home operation, zoned R-1

North: Single-family dwelling, zoned R-1

East: Single-family dwellings fronting along Nelson Drive, zoned R-1

South: Single-family dwelling, zoned R-1

West: Across Windsor Road, Single-family dwellings, zoned R-1

EVAULATION

The applicant is requesting a special use permit per Section 10-3-34 (6) of the Zoning Ordinance to allow a "major family day home" within the R-1, Single Family Residential District. "Major family day homes" are defined in the Zoning Ordinance as: *A child day care program offered in the residence of the provider or the home of any of the children in care for six (6) through twelve (12) children under the age of thirteen (13), exclusive of any children who reside in the home, when at least one (1) child receives care for compensation.*

Currently, the applicant operates a "minor family day home," Tots 'N Toyland, at the property located at 1214 Windsor Road. A "minor family day home" is allowed through the home occupation permit process and is defined in the Zoning Ordinance as *a child day care program offered in the residence of the provider or the home of any of the children in care for one (1) through five (5) children under the age of thirteen (13), exclusive of any children who reside in the home, when at least one (1) child*

receives care for compensation. No conditions more restrictive than those imposed on residences occupied by a single-family shall be imposed on the day home. At this time, Tots 'N Toyland has four children enrolled in the program. The applicant is working with the Virginia Department of Social Services (VDSS) to become licensed as a major family day home and expand enrollment beyond the allowable five children.

The VDSS has strict requirements regarding issues such as safety, cleanliness, play time, and floor area that individuals must meet in order to obtain a license to provide child care in a home. Approval and licensing from the VDSS does not, however, exempt an applicant from maintaining compliance with local ordinances or laws. Approval of this special use permit would allow for the applicant to operate as a major family day home, with the proper licensing from the VDSS for more than five children.

The applicant has described the facility as having an indoor activity and sleeping area of approximately 714 square feet along with an additional 400 square feet of outdoor play area. Parking is available at the top of the drive way where there is room for three cars at a time without blocking one another in. Hours of operation for the major family day home are 7:30 am until 6:00 pm, Monday thru Friday. Staff recognizes that the size of the facility and hours of operation are not conditions of the SUP and may change at times.

When operating a minor family day home with a Home Occupation Permit, care providers must abide by the regulations set forth in the permit; therefore, no person outside the family members residing on the premises shall be employed by the business. If the SUP is approved for a major family day home, the applicant may hire outside employees to work at the business. The applicant has stated that she intends to hire help after becoming licensed.

Windsor Road is a narrow cul-de-sac and does not have curb, gutter, or sidewalk along either side. While parking is permitted along the street, staff expressed concern to the applicant about cars parking along the shoulder of the road and impeding traffic. The applicant has stated that parents arrive at different times and they will generally use the driveway to drop-off and pick-up children. If needed the garage can be use for parking.

Staff does not foresee any negative impact in approving this application. Staff supports approval of the special use permit with the following condition:

1. If in the opinion of Planning Commission or City Council, parking becomes a nuisance, the special use permit can be recalled for further review, which could lead to the need for additional conditions, restrictions, or the revocation of the permit.

Date Application Received: 4-9-14

Application for Special Use Permit City of Harrisonburg, Virginia

Fee: \$375.00 plus \$30.00 per acre Total Paid: \$ 405⁰⁰ pd CB

Property Owner's Name: Erica Lynn Dorsey

Street Address: 1214 Windsor Road Email: edorsey46@gmail.com

City: Harrisonburg State: Virginia Zip: 22801

Telephone: Work 5404214754 Fax n/a Mobile 540-282-7453

Owner's Representative: N/A

Street Address: _____ Email: _____

City: _____ State: _____ Zip: _____

Telephone: Work _____ Fax _____ Mobile _____

Description of Property and Request

Location (Street Address): 1214 Windsor Road

Tax Map Number Sheet: 84 Block: 5 E Lot: 4 Lot Area: 105x170

Existing Zoning Classification: R1

Special Use being requested: Family Day Home 10-3-34(6)

Please provide a detailed description of the proposed (use additional pages may be attached): _____

The single-family home located at 1214 Windsor Road in Harrisonburg, VA on a dead end street surrounded by large shady oak trees, has 4,000 square feet of living space. The formal living room (24'6"x20'2"), (continued...)

Names and Addresses of Adjacent Property Owners (Use separate sheet for additional names)

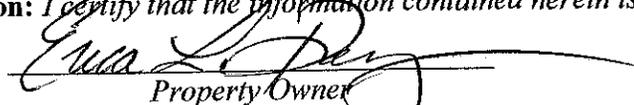
North: Hurl Lambert + Karen Evans - 1212 Windsor Road, Harrisonburg, VA 22801

South: Roger Allen Jones - 1216 Windsor Road, Harrisonburg, VA 22801

East: Gerald + Linda Hopkins - 1142 Nelson Drive, Harrisonburg, VA 22801

West: Daniel + Rebecca Fulcher - 1215 Windsor Road, Harrisonburg, VA 22801

Certification: I certify that the information contained herein is true and accurate.

Signature: 
Property Owner

ITEMS REQUIRED FOR SUBMISSION

- Completed Application
- Site Plan
- Description of Proposed Use
- Adjacent Property Owners

- Fees Paid
- Property Located on Tax Map
- _____
- _____

Continued:

adjoining full bath (7x7'6") and bedroom (12'6x13'6) make up 714 square feet and have been converted into a preschool that is filled with educational toys and books to teach and care for children ages 1-5. There is an additional 900 square feet of outdoor play space. There will be two caregivers of which one is the homeowner Erica Dorsey. Erica is a mother of three and a current member of the local fire department, Hose Company 4 where she volunteers as an Emergency Medical Technician and Firefighter. Erica Dorsey has fifteen years of experience caring for children and has a desire to make a positive impact on the lives of people around her. Erica has extensive experience with special needs children including Autism Spectrum Disorder and Epilepsy. Erica has three years of experience with early childhood development. She is skilled at leading children and fostering a strong and safe environment for which the children can flourish. Erica will hire an assistant to start upon licensing.

The preschool has been registered as Tots 'N Toyland LLC and currently has four children enrolled. The preschool is decorated in vibrant colors and soft diffused lighting. There are paintings of large fifteen foot trees with birds, owls and a tire swing. There is a large 3D kite positioned high in sky with flying butterflies. There are alphabet letters and numbers filling the walls and shapes and color charts too. There is a large oval table that can seat twelve children, an indoor slide, a train table, a ball-a-palooza, a soft-climb cushion block and a small tots area with a tool table, tunnel, activity desk and an accessible bin wall with age appropriate toys.

The sleeping room is equipped with four full size toddler mattresses and a portable crib for a younger child. The room is designed to allow for sleep including room darkening curtains and calming colors and decor. Additionally the changing tables and children's cubbies are located in this room and can accommodate up to twelve cubbies for children. The bathroom has a small potty seat for older children and an independent children's potty for beginners.

The outdoor play area has 3 swings, a slide, a water table, a tire climb, a sandbox, a basketball goal and many bikes and trucks and assorted toys. It is shaded from the sun and fully fenced in with a 4 foot vinyl privacy fence. It has the approved kiddie mat ground cover and is easily accessible due to a wide pathway that was recently installed.

The daily curriculum consists of activities that focus on gross motor skills, fine motor skills, socialization, expressive language, pragmatic language, participation, transition, tolerance, sharing, education and memorization, preparation for kindergarten, exploration, creativity and following the directives of the caregivers. There will be adequate outdoor time and rest time and a nutritional meal plan has been developed to include organic or all natural food that is without harmful artificial properties, chemicals, additives, preservatives, or modified ingredients.

Safety is paramount and no detail has been left unattended. An evacuation plan is posted, fire extinguisher is present, smoke alarms are in working order and a first aid kit is fully stocked. Hand washing will be regularly scheduled, food will be properly cooked, stored

Names and Addresses of Adjacent Property Owners Continued:

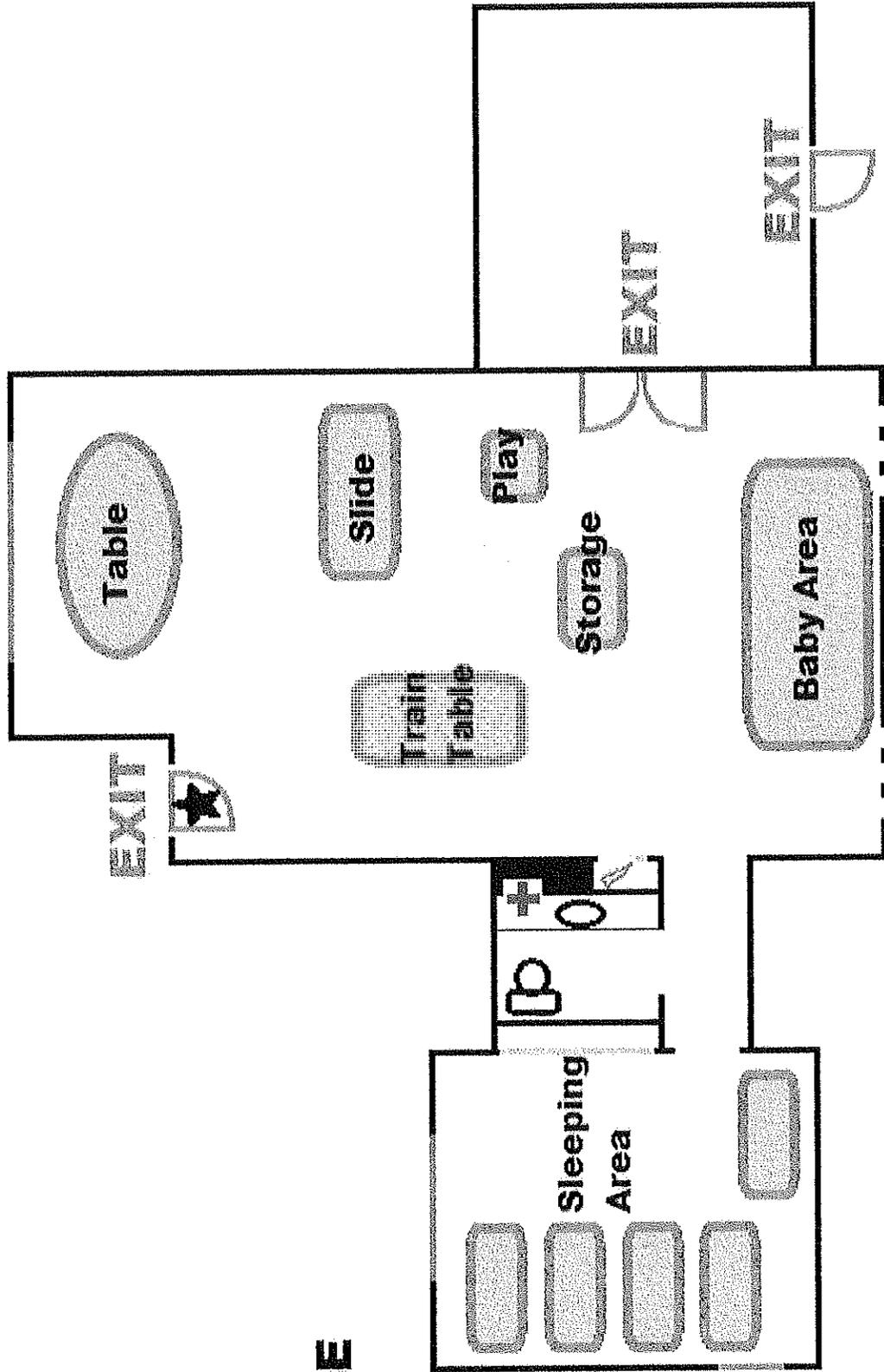
West: John and Barbara Mckee - 1213 Windsor Road, Harrisonburg, VA 22801

East: Andrew and Martha Waligora - 1151 Nelson Drive, Harrisonburg, VA 22801

Indoor preschool
Floorplan

W

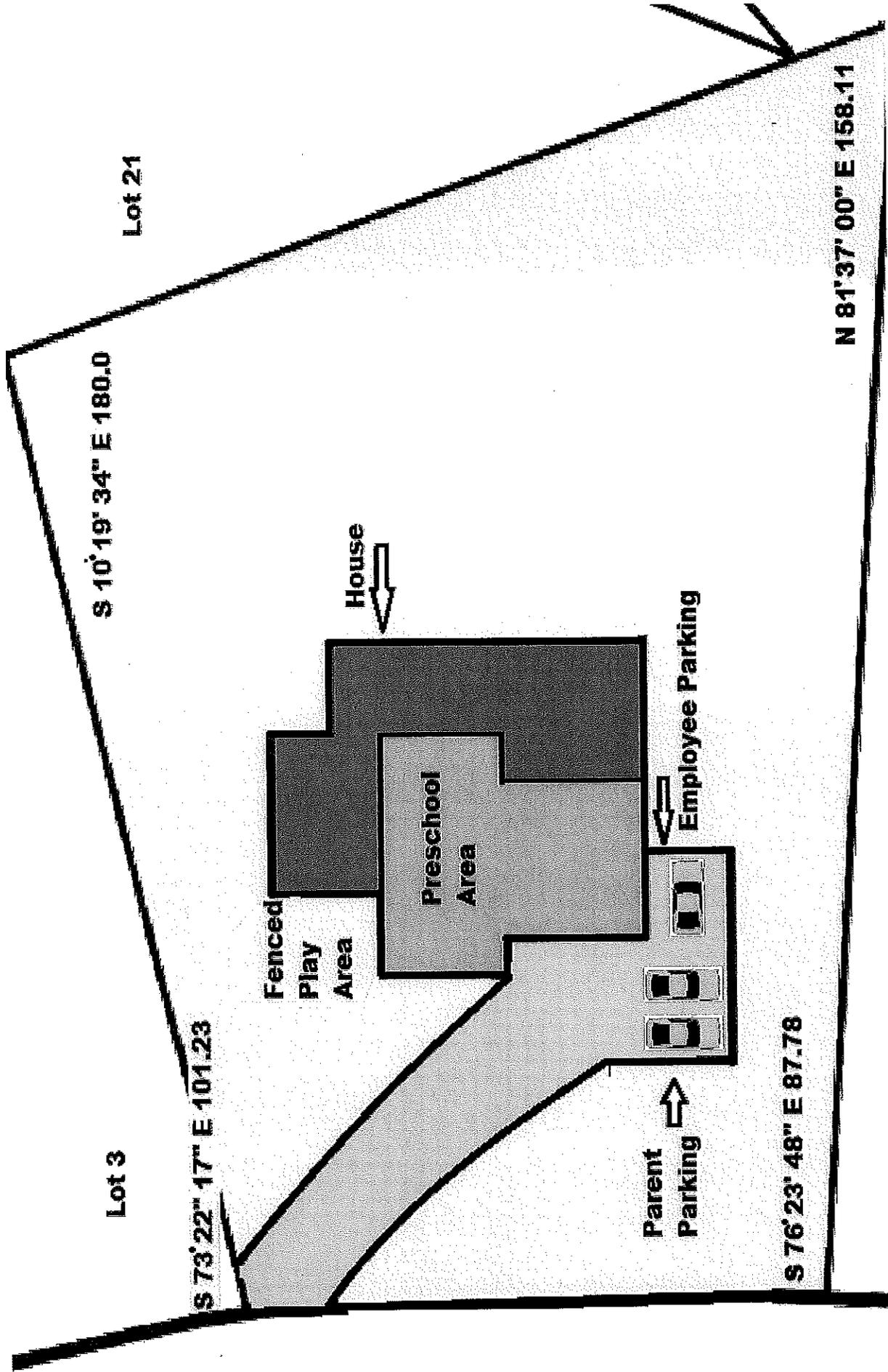
Front of House



E

N

S



Lot 21

S 10° 19' 34" E 180.0

N 81° 37' 00" E 158.11

House

Fenced Play Area

Preschool Area

Employee Parking

Parent Parking

Lot 3

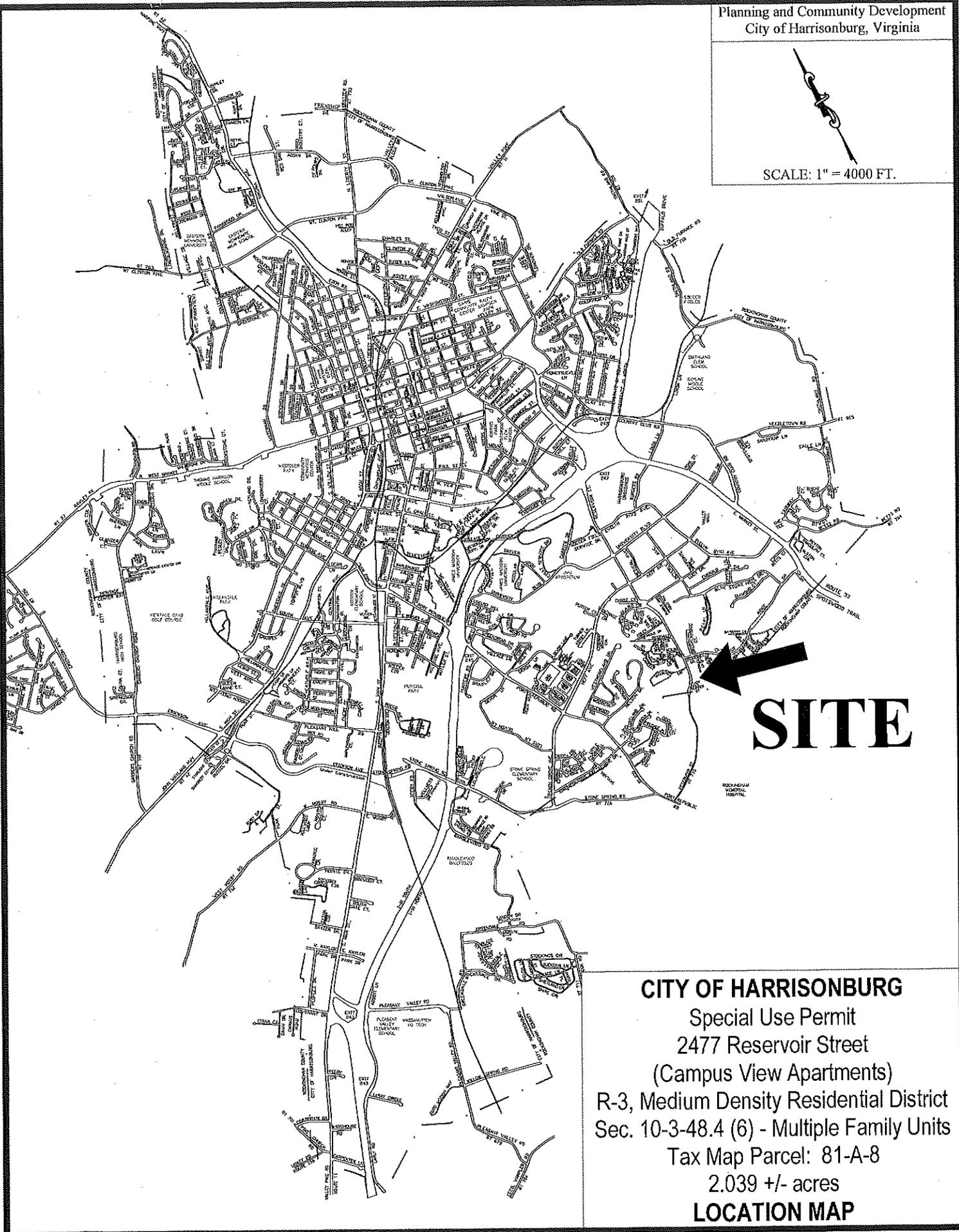
S 73° 22' 17" E 101.23

S 76° 23' 48" E 87.78

Windsor Road

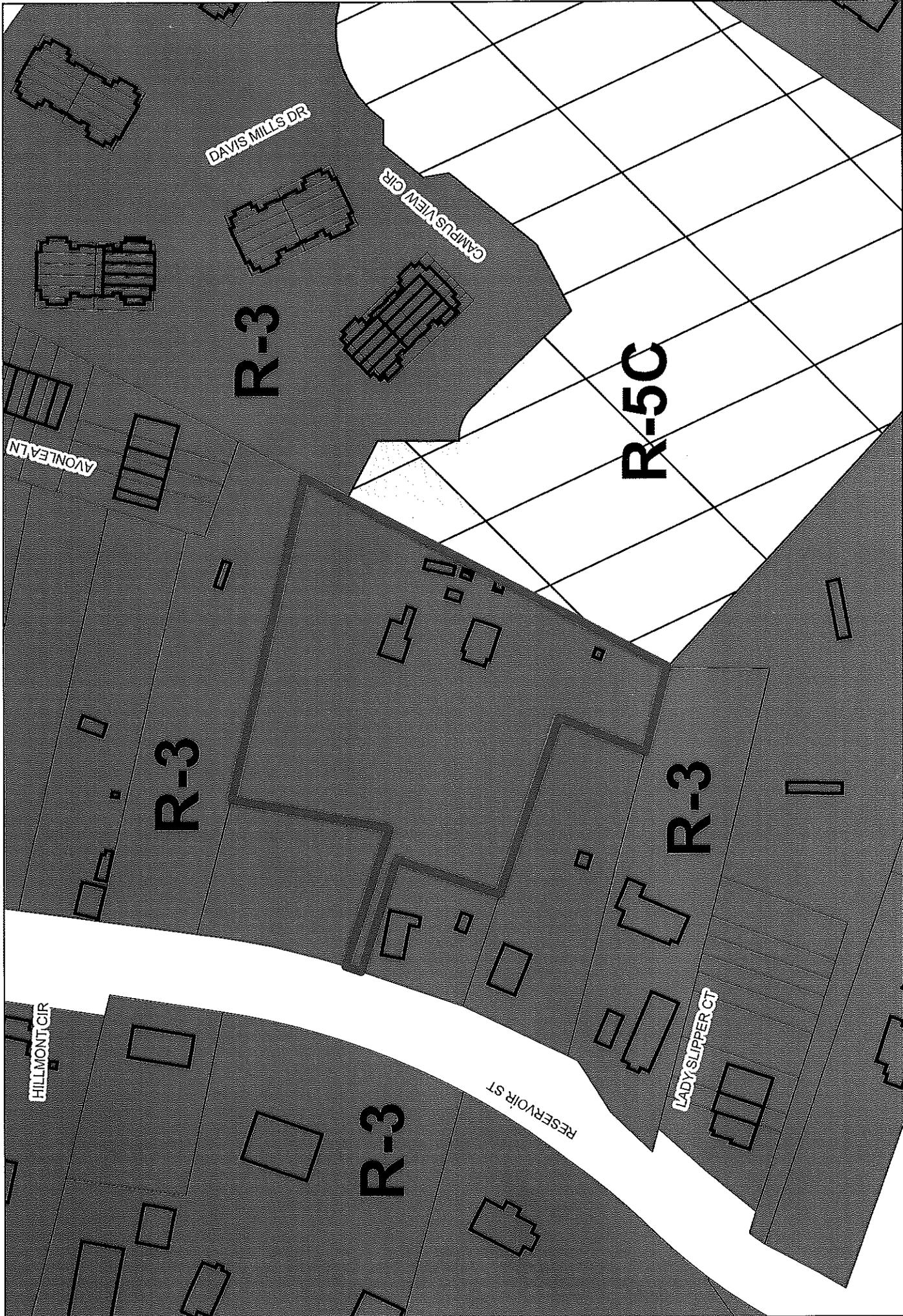


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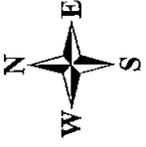


SITE

CITY OF HARRISONBURG
Special Use Permit
2477 Reservoir Street
(Campus View Apartments)
R-3, Medium Density Residential District
Sec. 10-3-48.4 (6) - Multiple Family Units
Tax Map Parcel: 81-A-8
2.039 +/- acres
LOCATION MAP



**S.U.P. - Multiple Family Units (Section 10-3-48.4 (6))
2477 Reservoir Street (Campus View Apartments)**





City of Harrisonburg, Virginia

DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT

STAFF REPORT

June 11, 2014

SUP – 2477 RESERVOIR STREET CAMPUS VIEW APARTMENTS

GENERAL INFORMATION

- Applicant:** Carmel Quinn Falls, Leon Nelson, and HGC, Inc. with representative Ed Blackwell of Blackwell Engineering
- Tax Map:** 81-A-8
- Acreage:** 2.039 +/- acres
- Location:** 2477 Reservoir Street
- Request:** Public hearing to consider a request for a special use permit within the R-3, Medium Density Residential District per Section 10-3-48.4 (6) of the Zoning Ordinance to allow multiple family dwellings of up to 12 units per building.

LAND USE, ZONING, AND SITE CHARACTERISTICS

The Comprehensive Plan designates this area as Medium Density Mixed Residential. This designation states that these largely undeveloped areas continue the existing medium density character of adjacent areas, but in a different form. They are planned for small-lot single family detached and single family attached neighborhoods where green spaces are integral design features. Apartments could also be permitted under special circumstances. They should be planned communities that exhibit the same innovative features as described for the low density version of mixed residential development. The gross density of development in these areas should be in the range of 4 to 12 dwelling units per acre and commercial uses would be expected to have an intensity equivalent to a Floor Area Ratio of at least 0.4, although the City does not measure commercial intensity in that way.

The following land uses are located on and adjacent to the property:

- Site:** Single-family dwelling, zoned R-3
- North:** Vacant parcels, zoned R-3
- East:** Campus View Condominiums and Apartments, zoned R-3 and R-5C
- South:** Single-family dwellings, zoned R-3
- West:** Across Reservoir Street, Dogwood Commons Apartments, zoned R-3

EVALUATION

The applicant is requesting a special use permit per Section 10-3-48.4 (6) of the Zoning Ordinance to allow multiple-family dwellings within the R-3, Medium Density Residential

District. The property is located in the southeastern portion of the City along Reservoir Street, approximately 550 feet north of the City/County boundary. Currently, there is a single-family dwelling on the subject property.

The proposed development is shown to contain three structures; two 12-unit apartment buildings and a 1,440+/- square foot clubhouse facility. The apartment buildings are described as three stories, with four bedrooms in each dwelling; for a total of 96 bedrooms. Parking is provided throughout the remainder of the property. The applicant has demonstrated that a TIA is not needed with this project.

Although addressed as Reservoir Street, the subject property has only a 15-foot wide pipe stem out to the street. The majority of the site is situated 100 feet back from Reservoir Street; with vacant parcels between the site and street. At this time there are no plans to connect the proposed development to Reservoir Street, and as shown on the drawing all traffic would be routed into the existing Campus View complex which connects with Chestnut Ridge Drive. The applicant has expressed a desire to have a temporary construction entrance off Reservoir Street during the construction phase for this project and Reservoir Street widening; as well as a possible future gated, emergency access into the site from Reservoir Street. This can be worked out during the comprehensive site plan review phase.

The applicant has noted on the submitted plan that the property line between the existing Campus View Apartments and the proposed new apartments would be vacated during the comprehensive site plan review process. When the interior property line is vacated to create one lot the orientation of the front, side, and rear setbacks changes as well. Staff has discussed with the applicant that the setbacks should all conform to the existing phases of Campus View. Additionally, a shared parking agreement would not be necessary once the property line is vacated.

Per Section 10-3-48.6 (b), vegetative screening would be required along the southern property boundary where the parking lot is adjacent to single-family dwellings. The submitted drawing indicates the location of the screening which must be an evergreen hedge (six-foot ultimate height) or shrubs/trees planted a minimum of five feet on center so as to form a dense screen. As well, all required parking lot landscaping must be met; this will be reviewed during the comprehensive site plan review.

As part of the requirements for obtaining a special use permit to build multi-family units in the R-3 district, an applicant must substantiate that they have met several conditions to justify the development. Briefly, the conditions state that:

1. Existing multiple-family development, or land planned for multiple-family development according to the Land Use Guide, is located adjacent to, across the street from or in close proximity to the proposed development.
2. The applicant has demonstrated that adequate vehicular, transit, pedestrian and bicycle facilities currently serve the site, are planned to serve the site according to a city or state plan, will be provided by the applicant at time of development, or are not needed because of the circumstances of the proposal.
3. The applicant has demonstrated that the proposed multiple-family development's design is compatible with adjacent existing and planned single-family, duplex and townhouse development;

4. The applicant has shown that the site is environmentally suitable for multiple-family development.

The applicant submitted a document to attempt to address these issues as numbered above; in general the applicant states that:

1. The proposed development is adjacent to Phases I and II of Campus View. The property line between Phase II and proposed Phase III will be abandoned during site plan development creating one unified parcel.
2. There are two entrances on Chestnut Ridge Drive serving the existing Campus View Complex that will serve this proposed Phase III. Sidewalks and bicycle facilities will be included with the site plan development. A connection to Reservoir Street is also being considered in discussions with the City. However, actual construction of such a connection cannot be completed until the Reservoir Street widening project is completed.
3. Architectural design and landscaping will be similar to Campus View Phase I and II. In addition, screening shall be provided along the boundaries with two adjacent single-family detached home parcels.
4. The site has an average, existing grade, south-to-north slope of 8.4%. This slope is similar to or less than existing phases and there are no critical slopes.

Staff believes the proposed development meets the conditions set forth in Section 10-3-48.6 (e). Currently, there are apartment complexes along this portion of Reservoir Street as well as adjoining the site. Transit bus stops are located on Reservoir Street and Chestnut Ridge Drive adjacent to the development and pedestrian/bicycle facilities are planned for the widening of Reservoir Street. The gross density of the development is twelve units per acre, which does fall within the range of 4 to 12 dwelling units per acre suggested in the Medium Density Mixed Residential land use.

Staff does not have concerns with this proposed development and recommends in favor of the special use permit request.

June 4, 2014

Subject: Special Use Permit – Campus View – Phase III

Alison Banks
City of Harrisonburg - Community Development
409 S. Main St.
Harrisonburg, VA 22801

Dear Alison:

This letter is in response to one of the comments in your May 27, 2014 Comments related to the subject request for a Special Use Permit. Comment 9. asks for descriptions of how specific conditions of Section 10-3-48.6 (e) (1 – 4) are met. The referenced Code section is below with the descriptions shown in **bold** text.

Sec. 10-3-48.6. (e)

Multiple-family development special use permits may be approved if the following conditions as determined by city council are met:

(1) Existing multiple-family development, or land planned for multiple-family development according to the Land Use Guide in the Comprehensive Plan, is located adjacent to, across the street from, or in close proximity to the proposed multiple-family development.

The proposed development is adjacent to Phases I and II of Campus View Apartments. The property line between Phase II and Phase III will be abandoned during site plan development creating one unified parcel.

(2) The applicant has demonstrated that adequate vehicular, transit, pedestrian and bicycle facilities:

- Currently serve the site;

There are two entrances on Chestnut Ridge Drive serving the existing Campus View Apartments that will serve this proposed Phase III. Sidewalks and bicycle facilities will be included with the site plan development.

or

- Are planned to serve the site according to a city or state plan with reasonable expectation of construction within the timeframe of the need created by the development;

A connection to Reservoir Street is also being considered in discussions with Harrisonburg Public Works Department. However, actual construction of such a connection cannot be completed until the Reservoir Street widening project is completed – possibly in 2016.

or

- Will be provided by the applicant at the time of development;

or

- Are not needed because of the circumstances of the proposal.

(3) The applicant has demonstrated that the proposed multiple-family development's design is compatible with adjacent existing and planned single-family, duplex and townhouse development. Compatibility may be achieved through architectural design, site planning, landscaping and/or other measures that ensure that views from adjacent single-family, duplex and townhouse development and public streets are not dominated by large buildings, mechanical/electrical and utility equipment, service/refuse functions and parking lots or garages. **Architectural design and landscaping will be similar to Campus View Phases I and II. In addition, screening shall be provided along the boundaries with two adjacent single family detached home parcels.**

(4) The applicant has shown that the site is environmentally suitable for multiple-family development. There shall be adequate area within the site, or the development shall be designed, to accommodate buildings, roads and parking areas with minimal impact on steep slopes and floodplains.

The site has an average, existing-grade, south-to-north slope of 8.4%. This slope is similar to or less than existing Phases I and II. There are no critical (2.5:1) slopes.

After reviewing these responses, let us know if they are satisfactory in responding to your Comment 9.

Dick Johnson

Blackwell Engineering

540-432-9555 (Office)

540-434-7604 (Fax)

www.blackwellengineering.com

Date Application Received: 05-13-14

Application for Special Use Permit City of Harrisonburg, Virginia

Fee: \$375.00 plus \$30.00 per acre Total Paid: \$ 465.00

Property Owner's Name: Carmel Quinn Falls & Leon Nelson & HGC Inc.

Street Address: 2477 Reservoir Street Email: _____

City: Harrisonburg State: VA Zip: 22801

Telephone: Work _____ Fax _____ Mobile _____

Owner's Representative: Blackwell Engineering PLC/Ed Blackwell

Street Address: 566 E. Market Street Email: ed@blackwellengineering.com

City: Harrisonburg State: VA Zip: 22801

Telephone: Work 540-432-9555 Fax 540-434-7604 Mobile _____

Description of Property and Request

Location (Street Address): 2477 Reservoir St., Harrisonburg, VA 22801

Tax Map Number Sheet: 81 Block: A Lot: 8 Lot Area: 2.039

Existing Zoning Classification: R-3

Special Use being requested: Sec. 10-3-48.4(6) Multiple-family dwellings of up to twelve (12) units per building under conditions set forth in subsection 10-3-48.6(e) and any such other conditions deemed necessary by city council.

Please provide a detailed description of the proposed (use additional pages may be attached):
It is proposed to construct two quadraplex buildings, 12 units per building, four bedrooms per unit, similar to those constructed in the adjacent Campus View Condominiums.

Names and Addresses of Adjacent Property Owners (Use separate sheet for additional names)

North: See separate sheet

South: _____

East: _____

West: _____

Certification: *I certify that the information contained herein is true and accurate.*

Signature: 
Property Owner

ITEMS REQUIRED FOR SUBMISSION

- Completed Application
- Site Plan
- Description of Proposed Use
- Adjacent Property Owners

- Fees Paid
- Property Located on Tax Map
- _____
- _____

CAMPUS VIEW
 2477 RESERVOIR STREET
 B/E #2417

PROPERTY CONTAINING BUILDING TO BE DEMOLISHED

TM#	ZONING		OWNER	OWNER ADDRESS	CITY
	Current	Rezoned as:			
81 A 8	R-3	-	CARMELQUINN FALLS & LEON NELSON & HQC INC.	2477 RESERVOIR ST.	HARRISONBURG, VA 22801

ADJACENT PROPERTIES

TM#	ZONING	OWNER	OWNER ADDRESS	CITY
1	R-5C	DAVIS MILL LLC	P.O. BOX 54	SOMERSET, VA 22972
2	R-3	VIRGINIA M. MINICK	916 GREENDALE RD.	HARRISONBURG, VA 22801
3	R-3	BRENT L MUMBERT	2421 RESERVOIR ST.	HARRISONBURG, VA 22801
4	R-3	HARRY H & MARY L SHIFFLETT	2511 RESERVOIR ST.	HARRISONBURG, VA 22801
5	R-3	CITY OF HARRISONBURG	345 S. MAIN ST.	HARRISONBURG, VA 22801
6	R-3	DOGWOOD COMMONS LLC	2403 MASSANETTA SPRINGS RD.	HARRISONBURG, VA 22801
7	R-3	CITY OF HARRISONBURG	345 S. MAIN ST.	HARRISONBURG, VA 22801
8	R-3	ELEANOR F KING & MATTHEW WILCOX	21 SHENANDOAH AVE.	HARRISONBURG, VA 22801
9	R-3	DAVIS MILL LLC	P.O. BOX 54	SOMERSET, VA 22972

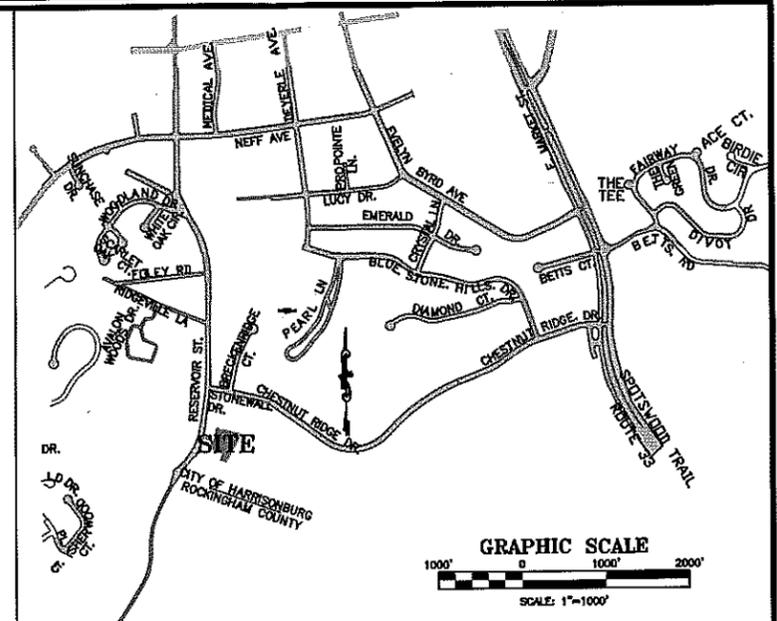
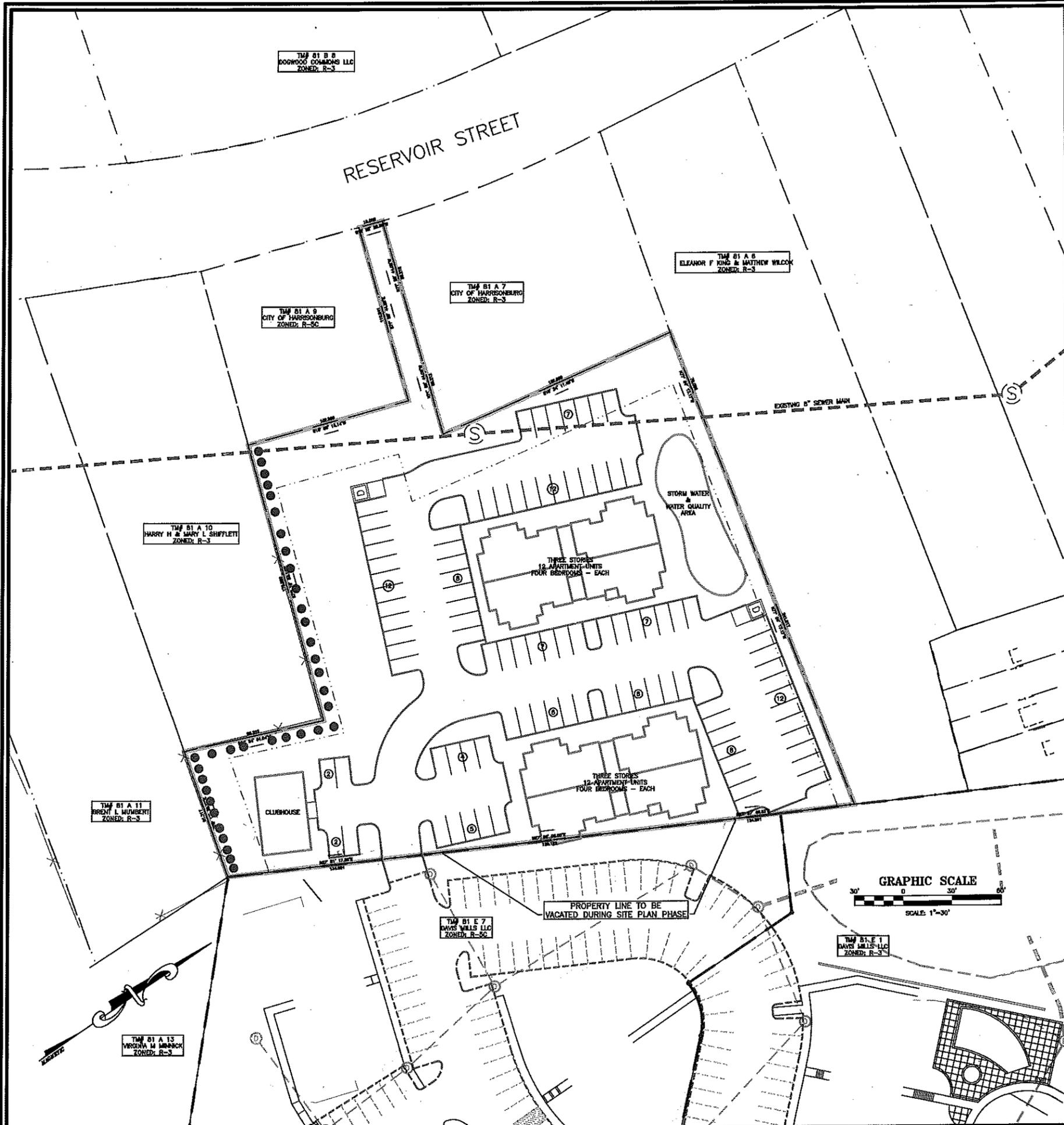
Trip Generation Table

LUC	Description	Size	Units	BR	Weekday Daily	AM Peak Hour			PM Peak Hour			Saturday Peak Hour		
						In	Out	Total	In	Out	Total	In	Out	Total
220-SH	Apartment - Student Housing	96			352	6	11	17	16	13	30	13	11	24
					0	0	0	0	0	0	0	0	0	0
					352	6	11	17	16	13	30	13	11	24
					352	6	11	17	16	13	30	13	11	24
					352	6	11	17	16	13	30	13	11	24
					352	6	11	17	16	13	30	13	11	24

Color Legend

	Equations for adjacent street
	Average rates for adjacent street - no formula given
	Formula for generator - no adjacent street data available
	Average rates for generator - no formula or adjacent street data available
	Substituted Weekday Daily and PM volumes for Saturday.

Total trips
 Total Capture
 Total Trips - Capture
 Total Pass-by Trips
 Total Net Trips



SITE DESIGN:
 BLACKWELL ENGINEERING
 ATTN: ED BLACKWELL
 566 EAST MARKET STREET
 HARRISONBURG, VA 22801
 540-432-8555

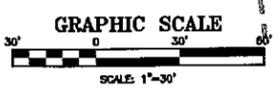
DEVELOPER:
 DAVIS MILLS LLC
 GUY BLUNDON
 P.O. BOX 54
 SOMERSET, VA 22972
 540-672-0825

PROPERTY INFO:
 TM# B1 A 6
 2.039± ACRES
 PROPOSED ZONING: R-3C
 PROPOSED USE:
 STUDENT HOUSING
 FEMA FLOOD ZONE X

BUILDING INFO:
 BUILDINGS #1 & #2:
 3 STORY QUADRAPLEX
 TOTAL AREA =
 5,788SF/FLOOR
 17,364SF/BUILDING
 UNIT HEIGHT = 34'
 NFF= 1000 OPW

LEGEND

- CENTER LINE
- CITY/COUNTY LINE
- SITE BOUNDARY
- ELECTRO/TELEPHONE
- METER/ TRANSFORMER
- GUY WIRE
- EXISTING UTILITY POLE
- EXISTING LIGHT POLES
- PROPOSED UTILITY POLE
- WATER LINES
- SANITARY SEWER FORCE MAIN
- SANITARY LINES
- SANITARY SEWER CLEANOUT
- STORM SYSTEM
- PROPOSED DITCH
- GAS LINES
- EXISTING PROPERTY LINE
- SETBACK LINE
- EASEMENT LINE
- EXISTING BUILDING
- PROPOSED BUILDING
- PROPOSED ROAD/POW
- PROPOSED PARKING
- EXISTING ROAD
- EXISTING PARKING
- CURBING: CO-5 OR CO-7
- CURBING: CO-2 OR CO-3
- HANDICAP PARKING
- CO-12/ASPHALT RAMP
- DUMPSTER
- EXISTING FIRE HYDRANT
- PROPOSED FIRE HYDRANT
- FIRE DEPARTMENT CONNECTION
- WATER VALVE
- WATER METER
- EXISTING FENCE LINE
- PROPOSED FENCE
- CONCRETE PAVING
- HEAVY PAVEMENT
- LIGHT PAVEMENT
- GRAVEL AREA
- GRASS AREA
- TRAFFIC FLOW
- REVISION SYMBOLS



PROPOSED USE AND PARKING REQUIREMENTS						
PROPOSED BUILDINGS	USE	SIZE, SF	RATE	UNITS	REQUIRED SPACES	REMARKS
2	MID-RISE APARTMENT 3 STORIES	5788/FL	3 1/2/UNIT	12/BUILDING	54	
	CLUBHOUSE	1440 SF	1 /250 SF	1	5	
					TOTAL REQUIRED SPACES	59
					HANDICAP REQUIRED	5
					TOTAL PROVIDED SPACES	108
					HANDICAP PROVIDED	5

* PER TABLE 17-124 (C) (1) ONE SPACE FOR EACH EMPLOYEE ON THE MAXIMUM WORKING SHIFT, PLUS ONE SPACE FOR EACH COMPANY VEHICLE OPERATING FROM THE PREMISES.

Date: MAY 2014
 Scale: 1" = 30'
 Designed by: EHB
 Drawn by: EHB
 Checked by: EHB

BLACKWELL ENGINEERING, PLC

566 East Market Street
 Harrisonburg, Virginia 22801
 PHONE: (540)432-8555 FAX: (540)434-7864
 E-Mail: BBE@blackwellengineering.com



Revision Dates

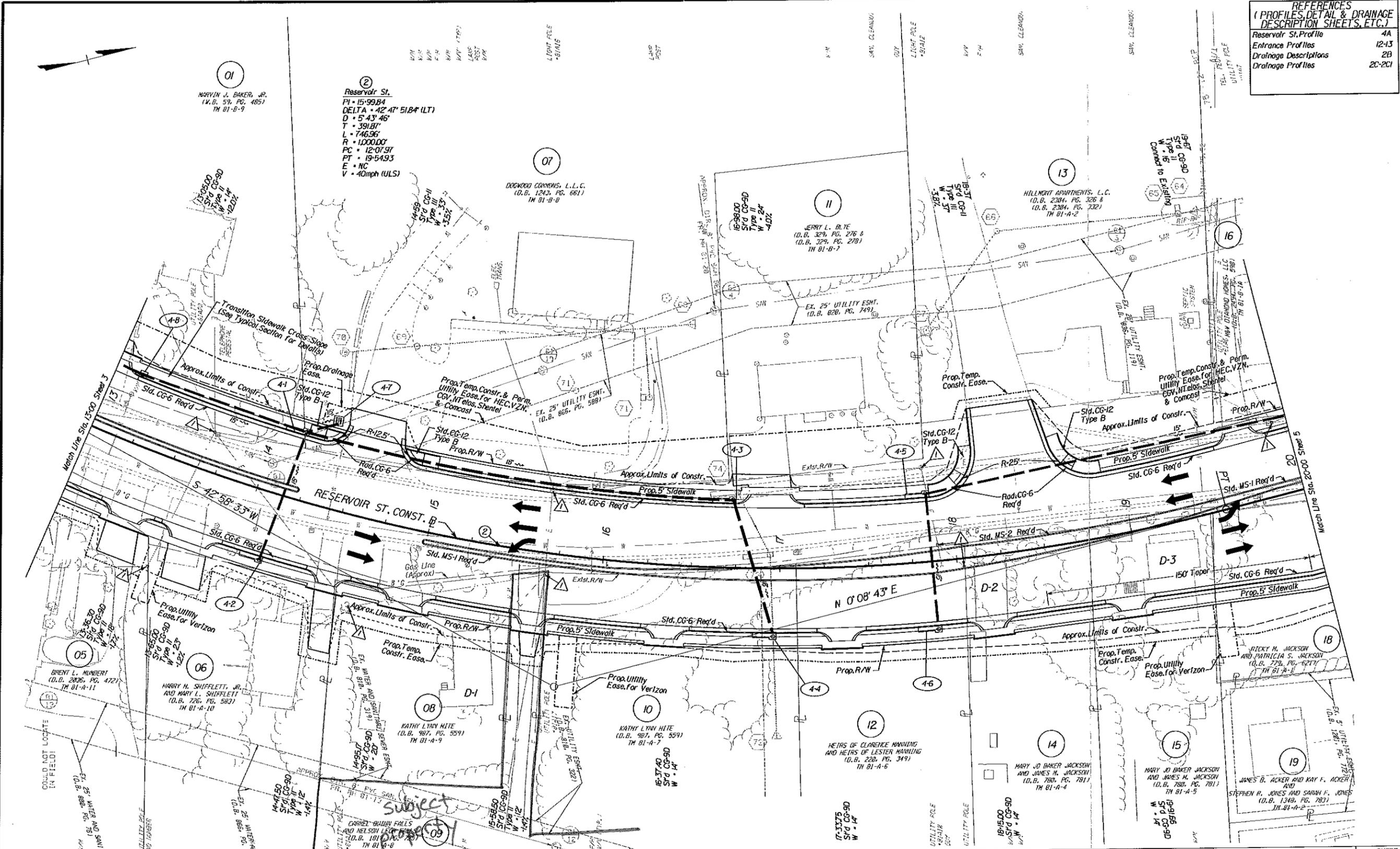
PHASE III - PRELIMINARY SITE PLAN

CAMPUS VIEW APARTMENTS
 DAVIS MILLS LLC
 P.O. BOX 54
 SOMERSET, VA 22972

Drawing No.
1
 of 1 Sheets

Job No. 2417

REFERENCES (PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.)	
Reservoir St. Profile	4A
Entrance Profiles	12-13
Drainage Descriptions	2B
Drainage Profiles	2C-2C1



THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION.

UTILITY LEGEND	
	Denotes Exst. Utility Pole to be Relocated by Others

	REV	DATE	DESCRIPTION	BY	SCALE: 1" = 25'	RESERVOIR STREET PLAN STA. 13+00-20+00 DIVISION OF ENGINEERING CITY OF HARRISONBURG 409 SOUTH MAIN STREET HARRISONBURG, VIRGINIA	SHEET 4
					DRAWN BY DATE		
					CHECKED BY DATE		
					DESIGN BY DATE		
					TAX MAP		

ORDINANCE AMENDMENT

**To Allow Junk Yards within the M-1,
General Industrial District by Special Use
Permit**



City of Harrisonburg, Virginia

DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT

STAFF REPORT

June 11, 2014

ZONING ORDINANCE AMENDMENT Amend Sections 10-3-96, 10-3-97, and 10-3-99

An application was submitted by Gerdau to amend the Zoning Ordinance to allow junk yards within the M-1, General Industrial District by special use permit. The Zoning Ordinance currently defines junk yards as *any space or area or portion of lots used for the storage, sale, keeping or abandonment of junk or waste materials, including used building material, for the dismantling, demolition, sale or abandonment of automobiles and other vehicles, machinery or parts thereof*. Currently, junk yards are prohibited in all zoning districts; however, there are some nonconforming junk yards within the City limits.

Gerdau is a leading producer of long steel in the Americas and one of the largest suppliers of special steel in the world. In addition, Gerdau operates 23 recycling facilities throughout North America. In March 2014 Richard Gallegos, a representative of Gerdau met with City staff about locating a scrap metal recycling yard in Harrisonburg. The facility would collect, sort, and process scrap materials such as old home appliances, vehicles and vehicle parts, industrial by-products and steel cans for later use in the steel-making operation. Basically, the recycle facility would function as a junk yard. City staff informed Gerdau that the proposed use would not be permitted within any current zoning classification unless an amendment was made to the Zoning Ordinance.

After working with City staff on proposed language, Gerdau submitted an application to amend the Zoning Ordinance Sections 10-3-96 (8) and (20) Uses permitted by right in the M-1, General Industrial District; Section 10-3-97 Uses permitted by special use permit in the M-1 district; and Section 10-3-99 (c) Other regulations in the M-1. Upon review of the application, staff is also proposing additional modifications to Section 10-3-24 Definitions and adding a parking requirement for junk yards under Section 10-3-25.

The applicant's proposed amendments would include adding text within Sections 10-3-96 (8) and (20) explaining that vehicle salvage, storage of inoperable vehicles, or sale of junk continues to be prohibited in association with vehicle and other equipment sales or storage and for uses associated with taxicab, limousine, and bus uses, unless the property owner obtains a special use permit allowing a junk yard. Junk yards would be added as an available special use within Section 10-3-97. And adding text within Section 10-3-99 (c) that could allow materials that are otherwise permitted (i.e. at junk yard operations) to not have to be completely enclosed within structures.

At the same time, staff is proposing a minor modification to the existing definition of "junk yard" for clarification of its meaning, and to add off-street parking regulations within Section 10-3-25 specifically for junk yards. Junk yards would be required to provide one parking space per

employee on the maximum shift, plus one space for every truck or vehicle used in association with the business and one space per 200 square feet of gross floor area.

Staff is supportive of the text changes for amendments to these sections. Staff believes that junk yards may be acceptable in some areas of the Industrial District under certain circumstances and with the ability to set conditions as warranted.

If the ordinance amendments are approved, Gerdau must then find an area of the City zoned M-1, General Industrial and apply for the special use permit. This application would require public hearings, notifications to adjoining property owners, advertising on the property and in the newspaper, and provides the opportunity to add conditions that City Council may deem appropriate. Thus staff does not foresee negative side effects in modifying the Zoning Ordinance for this purpose.

Staff recommends approval of the amendments and supports a favorable recommendation to City Council.

Date Application Received: 04-23-14

**Application for Ordinance Amendment
City of Harrisonburg, Virginia**

Fee: \$375.00

Total Paid: \$ previously paid on AP

Applicant's Name: Gerdau
Street Address: 11739 N. Valley Pike Email: _____
City: Broadway State: VA Zip: 22815
Telephone: Work _____ Fax _____ Mobile _____

Applicant's Representative: Richard Gallegos
Street Address: 11739 N. Valley Pike Email: richard.gallegos@gerdau.com
City: Broadway State: VA Zip: 22815
Telephone: Work 540-896-1544 Fax 540-981-0044 Mobile 540-597-9740

Description of Amendment

Zoning Ordinance Section: Sections 10-3-96, 10-3-97 and 10-3-99.

Proposed Text: Please see attached document with proposed text.

Certification: I certify that the information contained herein is true and accurate.

Signature: Richard Gallegos Gerdau Herb MRM
Applicant Signature

ITEMS REQUIRED FOR SUBMISSION

- Completed Application
- Ordinance Text
- Letter of description

- Fees Paid
-
-

Gerdau is a leading producer of long steel in the Americas and one of the largest suppliers of special steel in the world. With over 45,000 employees, it has industrial operations in 14 countries – in the Americas, Europe and Asia – which together represent an installed capacity of over 25 million metric tons of steel per year. It is the largest recycler in Latin America and around the world it transforms, each year, millions of metric tons of scrap into steel, reinforcing its commitment to sustainable development in regions where it operates. With more than 130,000 shareholders, the Company is listed on the stock exchanges of Sao Paulo, New York and Madrid.

Steel is the most recycled product in the world. Old home appliances, vehicle, vehicle parts, industrial by-products and steel cans, in short, objects that are discarded by society are transformed into raw material for steelmaking. Gerdau operates 23 recycling facilities that collect, sort, and process approximately two million tons of scrap annually into our steel-making operations. We currently rank as the second largest recycler of ferrous scrap in North America, consuming over seven million tons of scrap raw materials in our minimills annually.

Gerdau – Metallics Raw Materials is interested in operating a scrap metal recycling yard in the City of Harrisonburg to feed our Petersburg, Virginia Steel Mill. Current City of Harrisonburg zoning regulations do not allow for a scrap metal operation. We are proposing language to revise the code to allow for our business to operate in the City. Please reference the attached document with the proposed language revisions.

Suggested modifications to the Zoning Ordinance to allow junk yard operations in the City (deletions are stricken and additions are underlined):

Section 10-3-96. Uses Permitted By Right.

Delete language as shown.

- (8) Vehicles, recreation equipment, trailers, over the road tractors, their trailers, heavy equipment, manufactured homes, industrialized buildings, or agricultural equipment sales or storage served by a permanent building facility unless already incidental to an existing building. No vehicle salvage, storage of inoperable vehicles, or sale of junk is allowed unless a special use permit is approved allowing a junk yard.
- (20) Transportation service facilities, including but not limited to: taxicab, limousine, and bus. No vehicle salvage, storage of inoperable vehicles, or sale of junk is allowed unless a special use permit is approved allowing a junk yard.

Section 10-3-97. Uses Permitted By Special Use Permit.

Add new subsection as shown.

- (14) Junk yard, which shall be screened.

Section 10-3-99. Other Regulations.

Add language as shown.

- (c) Unless modified or superseded by other ordinances which directly apply to the general health, safety and welfare of the public, or unless otherwise permitted, all accessory storage or products to be processed or being processed, and supplies and waste materials resulting from such work, shall be completely enclosed within the structures of permanent and durable construction. In addition, all on-site refuse containers or refuse storage facilities shall be located within a designated area, screened, and meet the requirements for accessory buildings per section 10-3-114.

Below are other matters that should be understood.

Existing definition of “Junk Yard”: Any space or area or portion of lots used for the storage, sale, keeping or abandonment of junk or waste materials, including used building material, for the dismantling, demolition, sale or abandonment of automobiles and other vehicles, machinery or parts thereof.

ORDINANCE AMENDING AND RE-ENACTING SECTION

10-3-24

OF THE

CODE OF ORDINANCES

CITY OF HARRISONBURG, VIRGINIA

**Be it ordained by the Council of the City of
Harrisonburg, Virginia:**

That Section 10-3-24 Definitions shall be amended by modifying the definition as shown:

Junk Yard: Any space or area or portion of lots used for the storage, sale, keeping or abandonment of junk or waste materials, including used building material, or for the dismantling, demolition, sale or abandonment of automobiles and other vehicles, machinery or parts thereof.

The remainder of Section 10-3-24 is reaffirmed and reenacted in its entirety, except as hereby modified.

This ordinance shall be effective from the _____ day of _____, 2014.
Adopted and approved this _____ day of _____, 2014.

MAYOR

ATTESTE:

CLERK PRO TEMPORE

ORDINANCE AMENDING AND RE-ENACTING SECTION
10-3-25

OF THE
CODE OF ORDINANCES
CITY OF HARRISONBURG, VIRGINIA

**Be it ordained by the Council of the City of
Harrisonburg, Virginia:**

That Section 10-3-25 Off-Street Parking Regulations shall be amended by adding subsection (26) as shown:

(26) Junk yard: One (1) parking space per employee on the maximum shift plus one (1) parking space for every truck or other vehicle used in connection therewith and one (1) parking space per 200 square feet of gross floor area.

The remainder of Section 10-3-25 is reaffirmed and reenacted in its entirety, except as hereby modified.

This ordinance shall be effective from the ____ day of _____, 2014.
Adopted and approved this ____ day of _____, 2014.

MAYOR

ATTESTE:

CLERK PRO TEMPORE

ORDINANCE AMENDING AND RE-ENACTING SECTION

10-3-96

OF THE

CODE OF ORDINANCES

CITY OF HARRISONBURG, VIRGINIA

**Be it ordained by the Council of the City of
Harrisonburg, Virginia:**

That Section 10-3-96 Uses Permitted By Right of the M-1, General Industrial District shall be amended by deleting text within subsections (8) and (20) as shown:

- (8) Vehicles, recreation equipment, trailers, over the road tractors, their trailers, heavy equipment, manufactured homes, industrialized buildings, or agricultural equipment sales or storage served by a permanent building facility unless already incidental to an existing building. No vehicle salvage, storage of inoperable vehicles, or sale of junk is allowed unless a special use permit is approved allowing a junk yard.
- (20) Transportation service facilities, including but not limited to: taxicab, limousine, and bus. No vehicle salvage, storage of inoperable vehicles, or sale of junk is allowed unless a special use permit is approved allowing a junk yard.

The remainder of Section 10-3-96 is reaffirmed and reenacted in its entirety, except as hereby modified.

This ordinance shall be effective from the _____ day of _____, 2014.
Adopted and approved this _____ day of _____, 2014.

MAYOR

ATTESTE:

CLERK PRO TEMPORE

ORDINANCE AMENDING AND RE-ENACTING SECTION

10-3-97

OF THE

CODE OF ORDINANCES

CITY OF HARRISONBURG, VIRGINIA

**Be it ordained by the Council of the City of
Harrisonburg, Virginia:**

That Section 10-3-97 Uses Permitted By Special Use Permit of the M-1, General Industrial District shall be amended by adding subsection (14) as shown:

(14) Junk yard, which shall be screened.

The remainder of Section 10-3-96 is reaffirmed and reenacted in its entirety, except as hereby modified.

This ordinance shall be effective from the _____ day of _____, 2014.
Adopted and approved this _____ day of _____, 2014.

MAYOR

ATTESTE:

CLERK PRO TEMPORE

ORDINANCE AMENDING AND RE-ENACTING SECTION

10-3-99

OF THE

CODE OF ORDINANCES

CITY OF HARRISONBURG, VIRGINIA

**Be it ordained by the Council of the City of
Harrisonburg, Virginia:**

That Section 10-3-99 Other Regulations of the M-1, General Industrial District shall be amended by adding text within subsection (c) as shown:

- (c) Unless modified or superseded by other ordinances which directly apply to the general health, safety and welfare of the public, or unless otherwise permitted, all accessory storage or products to be processed or being processed, and supplies and waste materials resulting from such work, shall be completely enclosed within the structures of permanent and durable construction. In addition, all on-site refuse containers or refuse storage facilities shall be located within a designated area, screened, and meet the requirements for accessory buildings per section 10-3-114.

The remainder of Section 10-3-96 is reaffirmed and reenacted in its entirety, except as hereby modified.

This ordinance shall be effective from the _____ day of _____, 2014.
Adopted and approved this _____ day of _____, 2014.

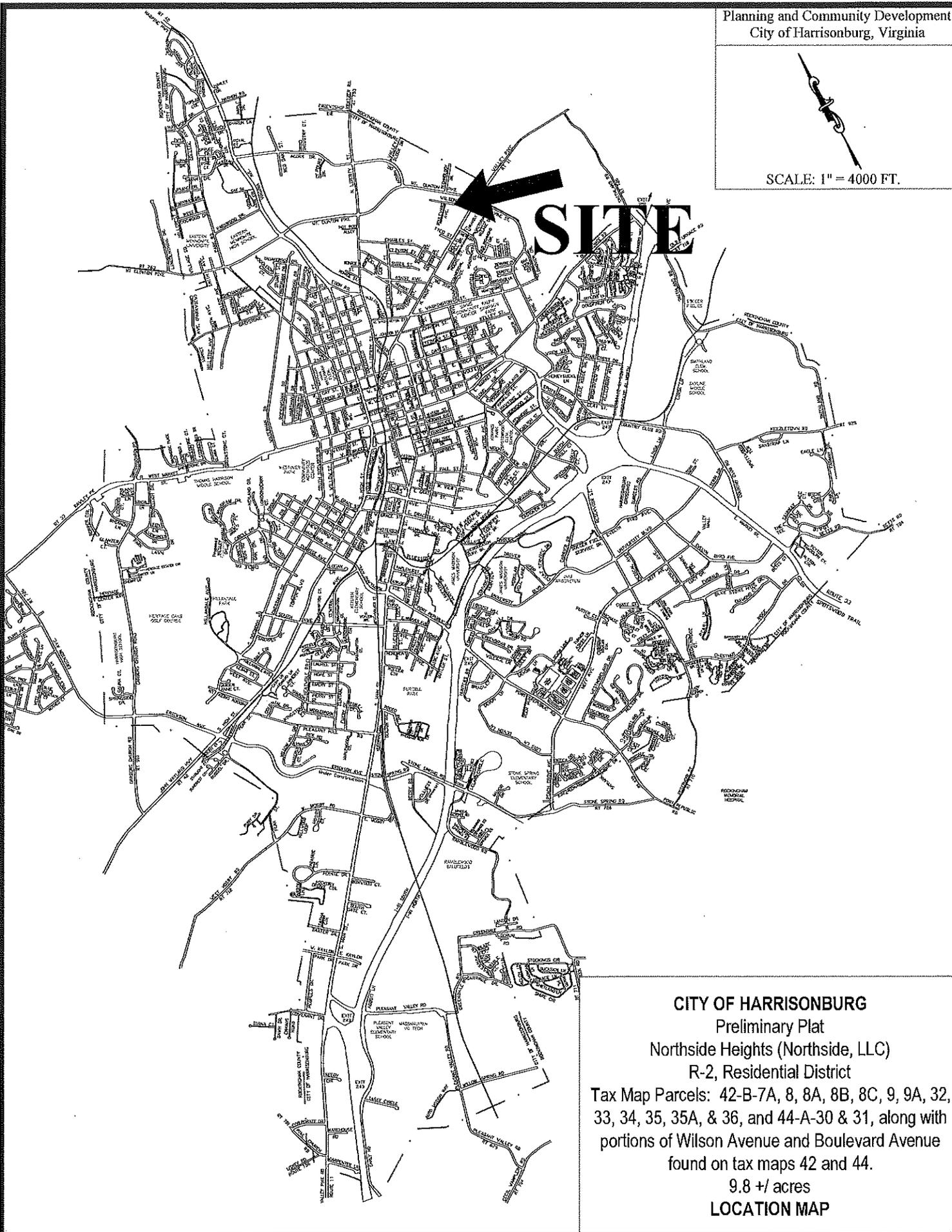
MAYOR

ATTESTE:

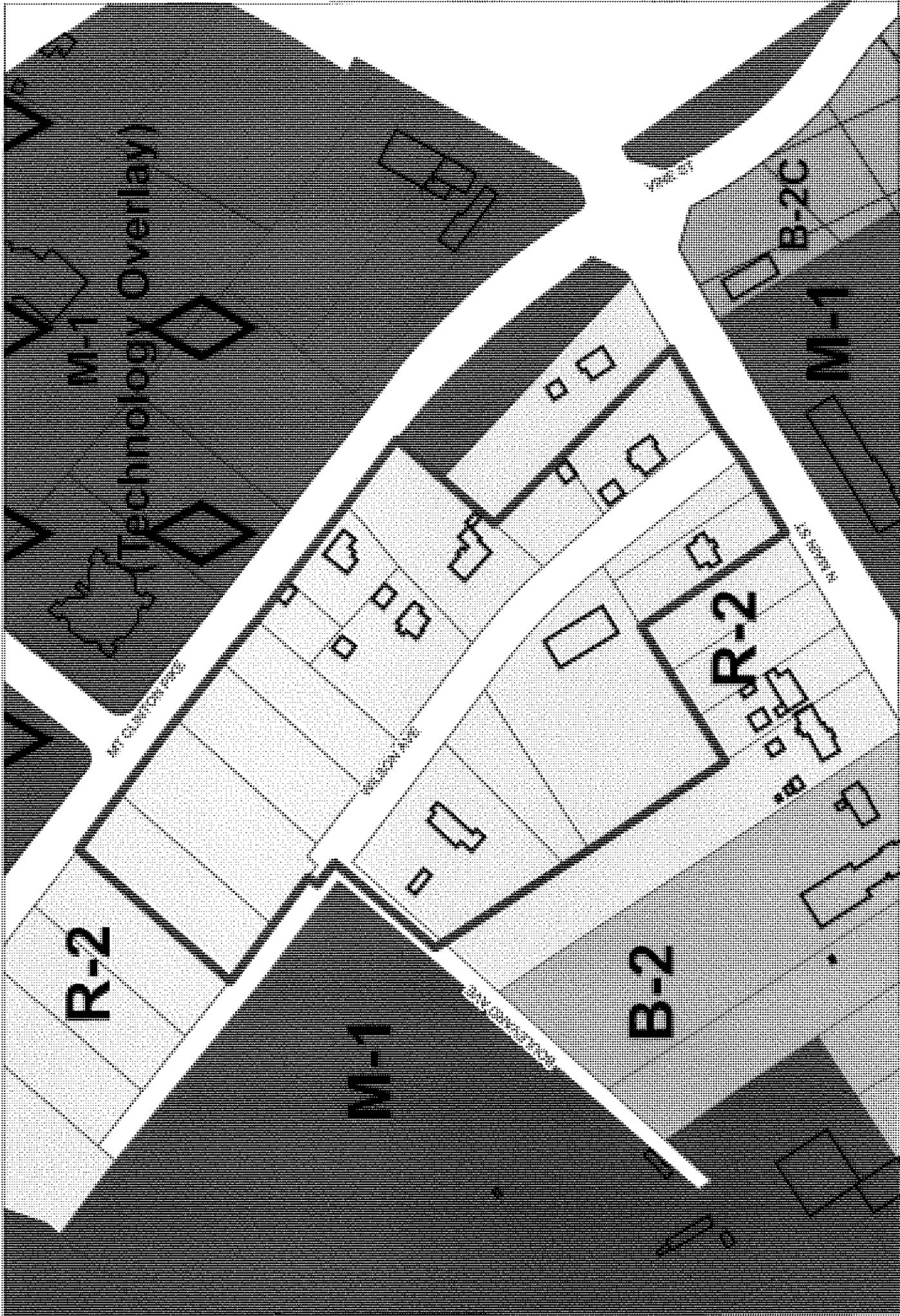
CLERK PRO TEMPORE



SCALE: 1" = 4000 FT.



CITY OF HARRISONBURG
Preliminary Plat
Northside Heights (Northside, LLC)
R-2, Residential District
Tax Map Parcels: 42-B-7A, 8, 8A, 8B, 8C, 9, 9A, 32,
33, 34, 35, 35A, & 36, and 44-A-30 & 31, along with
portions of Wilson Avenue and Boulevard Avenue
found on tax maps 42 and 44.
9.8 +/- acres
LOCATION MAP



**Preliminary Plat - Northside Heights
Northside, LLC and Joseph and Linda Moore**



City of Harrisonburg, Virginia

DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT

STAFF REPORT June 11, 2014

PRELIMINARY PLAT – NORTHSIDE HEIGHTS

GENERAL INFORMATION

- Applicant:** Northside, LLC and Joseph and Linda Moore
- Tax Map:** 42-B-7A, 8, 8A, 8B, 8C, 9, 9A, 32, 33, 34, 35, 35A, & 36, and 44-A-30 & 31 along with portions of Wilson Avenue and Boulevard Avenue found on tax map sheets 42 and 44.
- Acreage:** 9.8 +/- acres
- Location:** West Side of North Main Street and South Side of Mt. Clinton Pike
- Requests:** Consider a request to preliminarily subdivide 15 parcels and portions of existing public street right-of-way into eight parcels or four parcels. The plat is also preliminarily dedicating a 60-foot wide by 248-foot in length extension of Technology Drive and additional public street right-of-way along the south side of Mt. Clinton Pike.

LAND USE, ZONING, AND SITE CHARACTERISTICS

The Comprehensive Plan designates the majority of this area as General Industrial with portions designated as Commercial. The General Industrial designation states that these areas are composed of land and structures used for light and general manufacturing, wholesaling, warehousing, high-technology, research and development and related activities. The Commercial designation states that these areas include uses for retail, office, wholesale, or service functions. These areas are generally found along the City's major travel corridors and in the Central Business District of the City.

The following land uses are located on and adjacent to the property:

- Site:** Single Family homes, undeveloped lots, and portions of developed Wilson Avenue, and undeveloped Wilson Avenue and Boulevard Avenue, zoned R-2
- North:** Undeveloped parcels, zoned R-2, and across Mt. Clinton Pike, technology industrial park uses and undeveloped lots, zoned M-1 with the Technology Overlay
- East:** Undeveloped parcels, zoned R-2 and M-1, and across Mt. Clinton Pike parcels containing Harrisonburg Motor Express, zoned M-1
- South:** Single family homes, zoned R-2, and across North Main Street, industrial uses, zoned M-1
- West:** Single family home lot, zoned R-2, and property owned by GSW Investors with operations of Rockingham Construction and Special Fleet Service, Inc.

EVALUATION

The applicants are requesting to preliminarily subdivide 15 parcels and portions of the existing public street right-of-way (ROW) of Wilson Avenue and Boulevard Avenue. In total, the property contains 9.8 +/- acres of property, where six parcels contain a single family dwelling on each and the remaining lots are undeveloped. The applicants are the same parties that requested to close portions of Wilson Avenue and Boulevard Avenue last month. (The street closing request was recommended for approval by staff and also supported by Planning Commission with a 6-0 vote on May 14th. The public hearing for the street closing is scheduled to be held by City Council on June 10th.) As explained last month, the preliminary plat is another step in a multi-step process the applicants are initiating so they can enter into contract with an interested buyer to construct what they hope is commercial development—conceptually, a grocery store. The rezoning request for the planned development is occurring simultaneously with this preliminary plat, but is explained in a separate staff report.

At this time, due to unknown circumstances as to how the property may need to be subdivided to finalize the sale of portions of the property to an interested commercial developer, the submitted preliminary plat demonstrates two separate versions of how the property could be subdivided. Version 1 includes subdividing the 15 parcels and the portions of the public street ROWs requested for closure last month into eight parcels, where each of the existing six single family dwellings would remain on individual lots; two parcels would remain undeveloped. All eight parcels would be zoned R-2. Version 2 includes subdividing the same area into four parcels. Lot 1 of version two is the parcel that would be zoned B-2C, if the requested rezoning is approved. Lots 2, 3, and 4 of version two would maintain their R-2 zoning.

Both versions incorporate the same public street ROW dedications. The first area includes a 60-foot wide by 248-foot long extension of Technology Drive on the south side of Mt. Clinton Pike. This ROW dedication aligns with existing Technology Drive on the north side of Mt. Clinton Pike. To the south, the ROW would adjoin a 30-foot in width, undeveloped portion of Wilson Avenue, which is the remnant section of Wilson Avenue that would remain in place as was described in last month's staff report for the street closing. Staff appreciates the applicants'/ developer's willingness to dedicate and build this extension of Technology Drive. If ultimately dedicated and built, the street would accommodate a standard temporary turnaround establishing the intent to extend Technology Drive for additional development in this area.

The second public street ROW area to be dedicated is along the subject property's Mt. Clinton Pike street frontage, which stretches 730 feet in length. After completing a traffic impact analysis (TIA) for the planned commercial development, the Department of Public Works, the Virginia Department of Transportation (VDOT), and the applicants' engineer determined that a depth of about 30 feet of ROW would be needed along this entire stretch to accommodate the public street improvements required by the planned development. (VDOT was involved in the TIA due to the site's location being within 3,000 feet of a State maintained road. Such a TIA is generally known as a Chapter 527 review, which is required by State Code.) The improvements include turn lanes, a taper lane, installation of infrastructure for a future traffic signal at the Mt. Clinton Pike/Technology Drive intersection, and a 10-foot wide shared use path for pedestrians and bicyclists. Although street improvements would also be required along the subject property's North Main Street frontage, ROW dedication in this area is not needed as there is already sufficient width for the required bicycle lane and sidewalk improvements.

As a reminder, and also explained in last month's street closing staff report, the eventual final plat that dedicates Technology Drive and the additional street ROW as herein described cannot be approved until Technology Drive and the other street improvements are built or a form of surety is accepted by the City to cover all public improvements.

Both versions of the preliminary plat show and describe all required public easements, along with demonstrating the easements that would eventually be established for the existing public water and sewer infrastructure and those for the overhead utility lines owned by HEC within and adjacent to the areas requested for closure of Wilson Avenue and Boulevard Avenue. The plat also illustrates a shared access easement for the existing single family homes that would be impacted by the street closings.

In summary, the plat meets all requirements of the Subdivision Ordinance, and therefore, staff recommends approving both versions of the preliminary plat.

Date Application Received: 05-09-14

Total Paid: \$335.⁰⁰ AF

Application for Preliminary Subdivision Plat Approval

City of Harrisonburg, Virginia

Fee: w/o Variance Request \$175.00 plus \$20.00 per lot Plus fees for TIA reviews where applicable (see back for details)
Variance Request \$200.00 plus \$20.00 per lot

I, James A. Patton, LS, hereby apply for preliminary subdivision plat approval for the following property located within the City of Harrisonburg:

Description of Property

Title of Subdivision: Northside Heights
Location (Street Address): S/W Corner - Mt. Clinton Pike/N Main St Sheet: 44 & 42 Block: _____ Lot: _____
Total Acreage: 10.65 Number of Lots Proposed: 8 or 4 AF Zoning Classification: R-2
9.8 AF

Proposed Use of Property: Retain current use - single family residential. Reconfigure parcels due to proposed road closures.
Dedication of easements for existing utilities due to proposed road closures. Dedication of 60' Public R/W for Technology Drive.

Property Owner's Name: Northside, LLC; Joseph H. Moore and Linde H. Moore
Street Address: 75 Wilson Avenue Email: JEMMAMI@AOL.COM
City: Harrisonburg State: VA Zip: 22801
Telephone: Work 540 828 7432 Fax _____ Mobile 540-405-4495

Owner's Representative (if applicable): Balzer and Associates, Inc.
Street Address: 1561 Commerce Road Suite 401 Email: jpatton@balzer.cc; wmoore@balzer.cc
City: Verona State: VA Zip: 24482
Telephone: Work 540-248-3220 Fax _____ Mobile _____

Developer: MVG Development
Telephone: 303-573-6500 Email: _____

Surveyor/Engineer: James A. Patton, LS; William S. Moore, PE
Telephone: 540-248-3220 Email: jpatton@balzer.cc; wmoore@balzer.cc

VARIANCES

NOTE: If a variance is requested, please provide the following information:
I (we) hereby apply for a variance from Section _____ of the City of Harrisonburg Subdivision Ordinance and/or Section _____ of the City of Harrisonburg Design and Construction Standards Manual, which require(s):

I (we) believe a variance should be granted based on the following "unnecessary hardship" which is peculiar to the property in question (See Section 10-2-2 of the Subdivision Ordinance):

The City of Harrisonburg's preliminary plat and subdivision requirements are in the code of the City of Harrisonburg, Subdivision Ordinance Sections 10-2-1 through 10-2-86. Please read these requirements carefully.

Certification: I have read the ordinance requirements. I also certify that the information contained herein is true and accurate.
Signature: James E. Moore Signature: _____
NorthSide LLC Property Owner Applicant, if different from owner

See Back for Additional Application Fees Regarding TIA Reviews
Last Updated: 07/01/2011

THE CONTOURS SHOWN HEREON ARE TAKEN FROM A FIELD SURVEY. THE ORIGINAL DATA WAS OBTAINED ON APRIL 2 THROUGH APRIL 11, 2014. THIS PRELIMINARY PLAT DOES NOT REPRESENT A COMPLETE BOUNDARY OR TOPOGRAPHIC SURVEY.

1. GENERAL INFORMATION
ACREAGE: 9.8 ACRES
PROPOSED USE: N/A
CURRENT ZONING: R-2
CURRENT USE: RESIDENTIAL

2. OWNERS:
NORTHSIDE, L.L.C.
75 WILSON AVENUE
HARRISONBURG, VA 22801

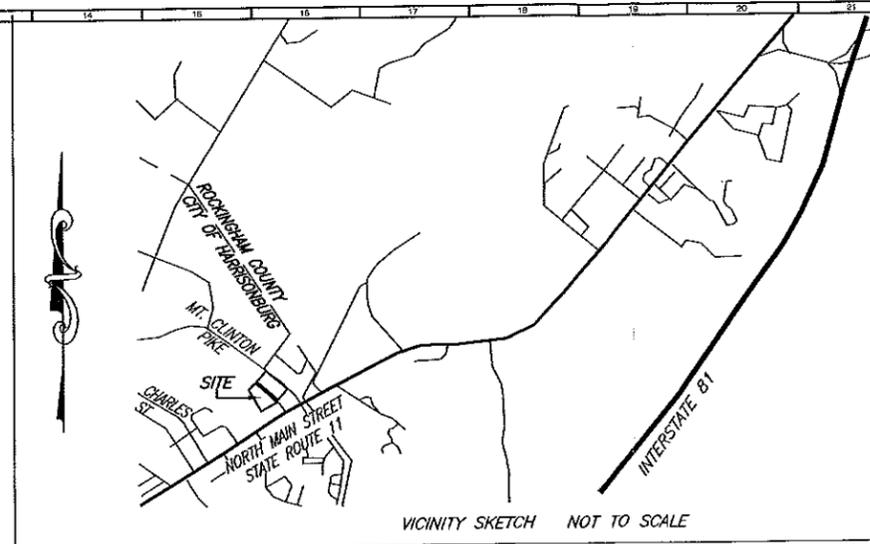
JOSEPH H. MOORE
LINDA H. MOORE
75 WILSON AVENUE
HARRISONBURG, VA 22801
(PARCELS 42-B-8A & 42-B-8B)

THERE ARE NO DEEDS OF TRUST ON THE SUBJECT PARCELS.

3. SUBDIVIDER/DESIGNER:
BALZER AND ASSOCIATES, INC.
1561 COMMERCE ROAD
SUITE 401
VERONA, VA 24482

4. EASEMENTS
PUBLIC EASEMENTS TO BE CREATED FOR EXISTING UTILITIES. A 10' PUBLIC GENERAL UTILITY EASEMENT WILL BE DEDICATED ALONG FRONT PROPERTY LINES THAT ARE ADJACENT TO PUBLIC STREET RIGHT OF WAY, AND 10 FOOT PUBLIC GENERAL UTILITY EASEMENTS WILL BE DEDICATED CENTERED ON SIDE PROPERTY LINES OR REAR PROPERTY LINES.

5. RIGHT OF WAY DEDICATION
THERE WILL BE SUFFICIENT RIGHT OF WAY DEDICATIONS TO HAVE AT LEAST TWO FEET OF RIGHT OF WAY BEHIND PHYSICAL STREET IMPROVEMENTS.



BALZER AND ASSOCIATES, INC.
REGISTERED PROFESSIONAL ENGINEERS AND SURVEYORS
1561 Commerce Road Suite 401
Verona, Virginia 24482
Phone: 540-298-0328
FAX: 540-248-2211
E-MAIL: info@balzer.com

15871 City View Drive
Suite 100
Midlothian, Virginia 23113
Phone: 804-991-0971
FAX: 804-991-0972

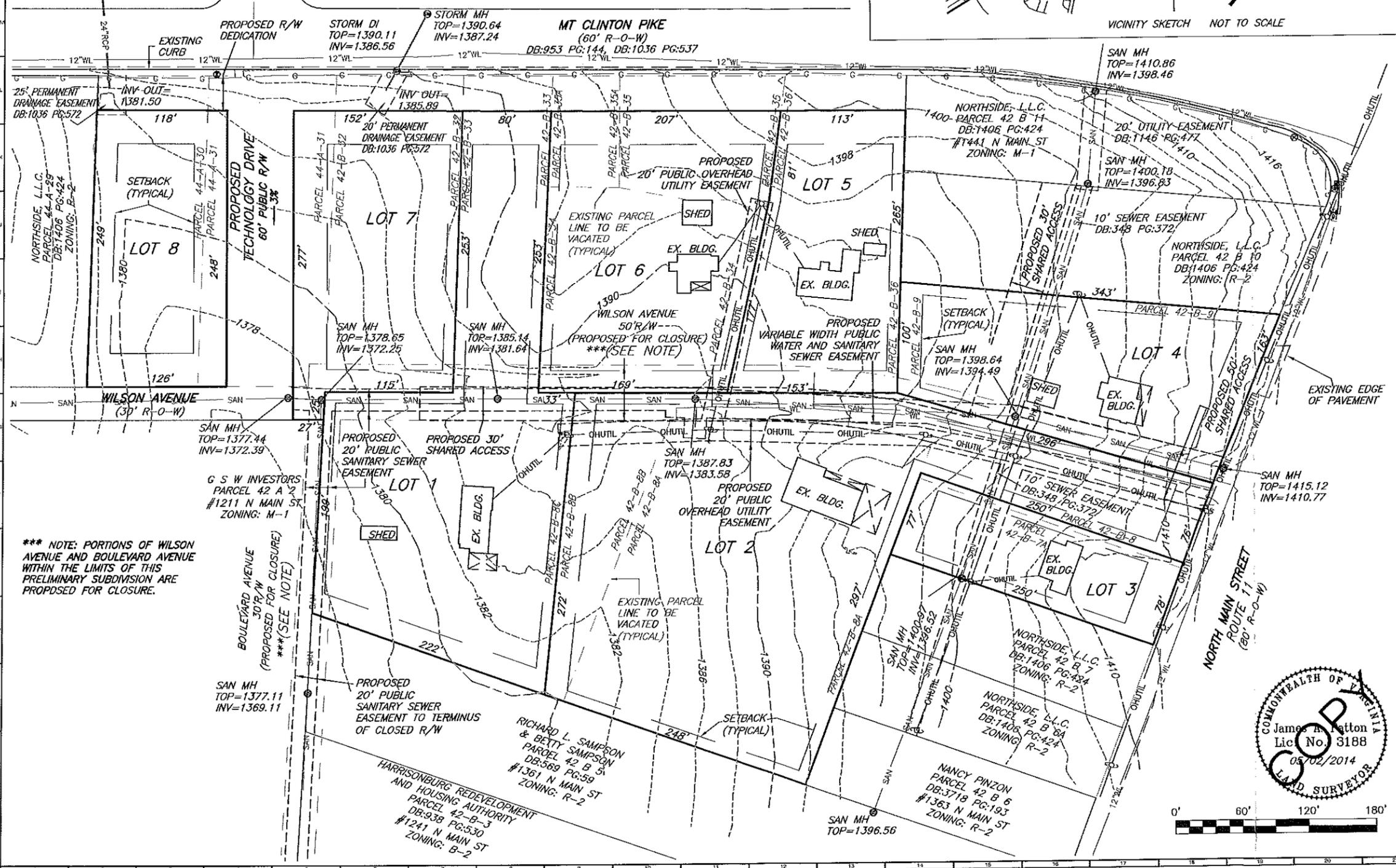
448 Poplar Ferry Road, Box 100
Chatham, Virginia 24553
Phone: 540-281-4220
FAX: 540-281-4294

**NORTHSIDE HEIGHTS
PRELIMINARY PLAT
VERSION 1**
CITY OF HARRISONBURG, VIRGINIA

DRAWN BY: JAP
DESIGNED BY:
CHECKED BY: WSM
DATE: 05/02/2014
REVISIONS:



SCALE: 1"=60'
SHEET NO.
1 OF 1
JOB NO.
S1400022.00



*** NOTE: PORTIONS OF WILSON AVENUE AND BOULEVARD AVENUE WITHIN THE LIMITS OF THIS PRELIMINARY SUBDIVISION ARE PROPOSED FOR CLOSURE.

BOULEVARD AVENUE 30' R/W (PROPOSED FOR CLOSURE) *** (SEE NOTE)

SAN MH TOP=1377.11 INV=1369.11

PROPOSED 20' PUBLIC SANITARY SEWER EASEMENT TO TERMINUS OF CLOSED R/W

RICHARD L. SAMPSON & BETTY SAMPSON PARCEL 42 B 5 #1361 N MAIN ST ZONING: R-2

HARRISONBURG REDEVELOPMENT AND HOUSING AUTHORITY PARCEL 42-B-3 #1241 N MAIN ST ZONING: B-2

NANCY PINZON PARCEL 42 B 6 #1363 N MAIN ST ZONING: R-2

NORTHSIDE, L.L.C. PARCEL 42 B 6A DB:1406 PG:424 ZONING: R-2

NORTHSIDE, L.L.C. PARCEL 42 B 7 DB:1406 PG:424 ZONING: R-2

NORTHSIDE, L.L.C. PARCEL 42 B 10 DB:1406 PG:424 ZONING: R-2

NORTHSIDE, L.L.C. PARCEL 42 B 11 DB:1406 PG:424 #1441 N MAIN ST ZONING: M-1

25' PERMANENT DRAINAGE EASEMENT DB:1036 PG:572

20' PERMANENT DRAINAGE EASEMENT DB:1036 PG:572

STORM DI TOP=1390.11 INV=1386.56

STORM MH TOP=1390.64 INV=1387.24

INV IN=1382.02

INV OUT=1381.50

INV OUT=1385.89

INV IN=1382.02

INV OUT=1381.50

INV OUT=1385.89

INV IN=1382.02

INV OUT=1381.50

INV OUT=1385.89

INV IN=1382.02

INV OUT=1381.50

INV OUT=1385.89

INV IN=1382.02

INV OUT=1381.50

INV OUT=1385.89

INV IN=1382.02

INV OUT=1381.50

INV OUT=1385.89

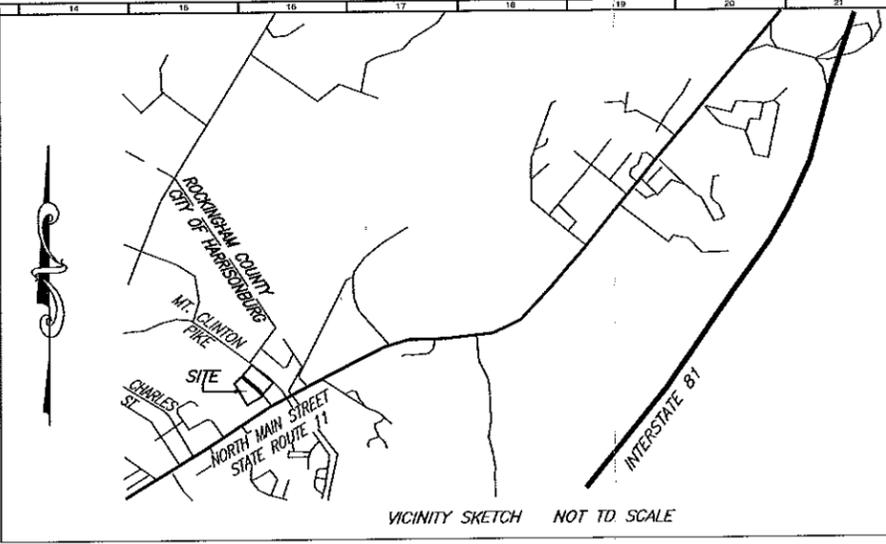
THE 2' INTERVAL CONTOURS SHOWN HEREON ARE TAKEN FROM A FIELD SURVEY. THE ORIGINAL DATA WAS OBTAINED ON APRIL 2 THROUGH APRIL 11, 2014. THIS PRELIMINARY PLAT DOES NOT REPRESENT A COMPLETE BOUNDARY OR TOPOGRAPHIC SURVEY.

- GENERAL INFORMATION**
ACREAGE: 9.8 ACRES
CURRENT ZONING: R-2
CURRENT USE: RESIDENTIAL
PROPOSED ZONING (LOT 1): B-2(CONDITIONAL)
PROPOSED USE (LOT 1): RETAIL
- OWNERS:**
NORTHSIDE, L.L.C.
75 WILSON AVENUE
HARRISONBURG, VA 22801

JOSEPH H. MOORE
LINDA H. MOORE
75 WILSON AVENUE
HARRISONBURG, VA 22801
(PARCELS 42-B-8A & 42-B-8B)

THERE ARE NO DEEDS OF TRUST ON THE SUBJECT PARCELS.

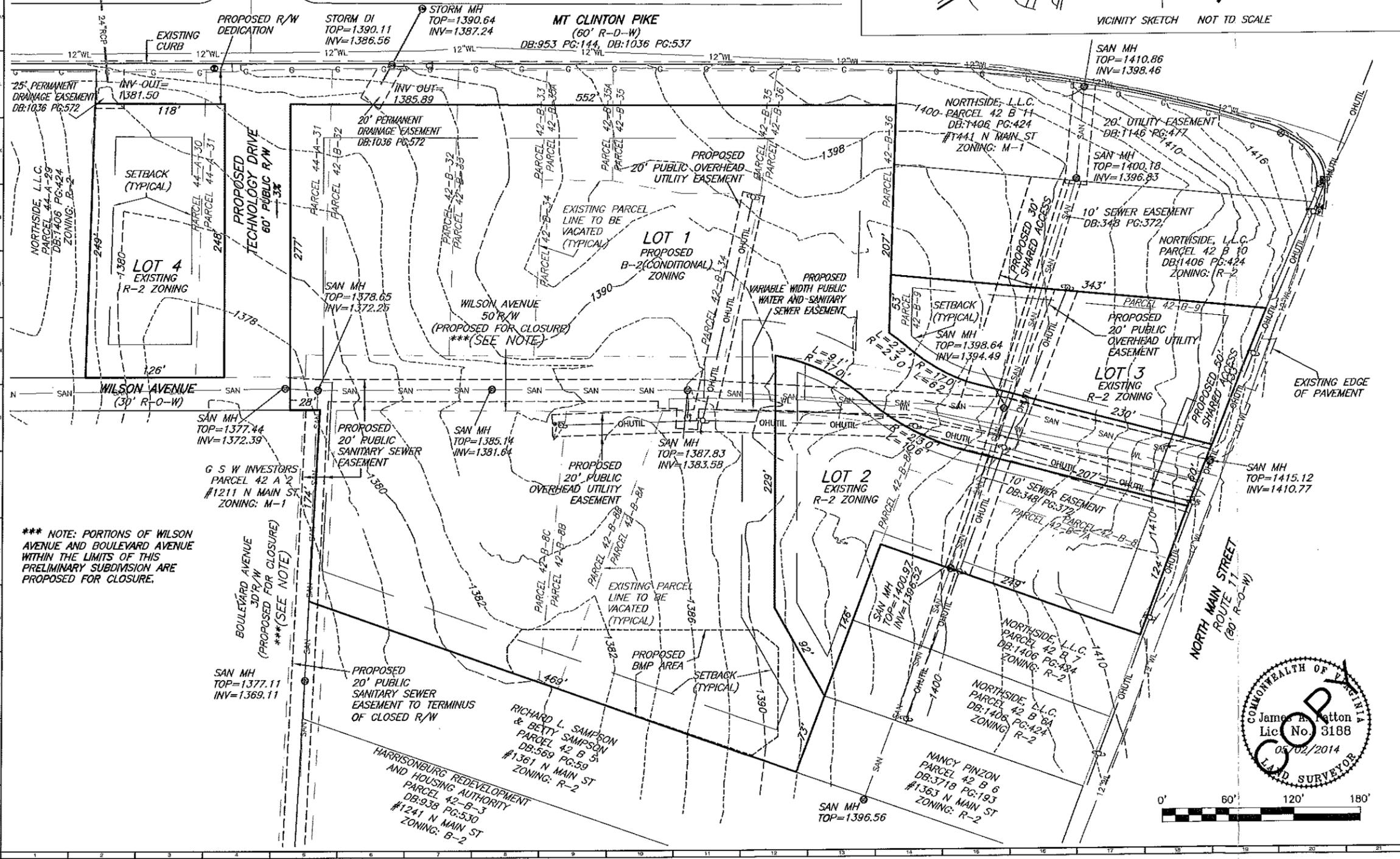
EXISTING BUILDINGS WITHIN THE LIMITS OF THIS SUBDIVISION WILL BE DEMOLISHED.
- SUBDIVIDER/DESIGNER:**
BALZER AND ASSOCIATES, INC.
1561 COMMERCE ROAD
SUITE 401
VERONA, VA 24482
- EASEMENTS**
PUBLIC EASEMENTS TO BE CREATED FOR ANY EXISTING UTILITIES THAT REMAIN.
A 10' PUBLIC GENERAL UTILITY EASEMENT WILL BE DEDICATED ALONG FRONT PROPERTY LINES THAT ARE ADJACENT TO PUBLIC STREET RIGHT OF WAY, AND 10 FOOT PUBLIC GENERAL UTILITY EASEMENTS WILL BE DEDICATED CENTERED ON SIDE PROPERTY LINES OR REAR PROPERTY LINES.
- RIGHT OF WAY DEDICATION**
THERE WILL BE SUFFICIENT RIGHT OF WAY DEDICATIONS TO HAVE AT LEAST TWO FEET OF RIGHT OF WAY BEHIND PHYSICAL STREET IMPROVEMENTS.



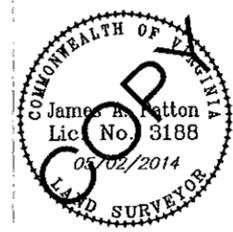
BALZER AND ASSOCIATES, INC.
REFLECTING TOMORROW
PLANNERS • ARCHITECTS
ENGINEERS • SURVEYORS
1561 Commerce Road Suite 401
Verona, Virginia 24482
Phone: 540/248-0220
FAX: 540/248-0221
EMAIL: hvance@balzer.co

1208 Corporate Oaks
Roanoke, VA 24014
Phone: 540/772-8500
FAX: 540/772-6026

15871 City View Drive
Suite 500
Martinsville, VA 22113
Phone: 540/984-6674
FAX: 540/984-6674
HARRISONBURG OFFICE
438 Poppers Ferry Road, 107
Harrisonburg, Virginia 22802
Phone: 540/248-0220
FAX: 540/248-0221



*** NOTE: PORTIONS OF WILSON AVENUE AND BOULEVARD AVENUE WITHIN THE LIMITS OF THIS PRELIMINARY SUBDIVISION ARE PROPOSED FOR CLOSURE.



**NORTHSIDE HEIGHTS
PRELIMINARY PLAT
VERSION 2**
CITY OF HARRISONBURG, VIRGINIA

DRAWN BY: JAP
DESIGNED BY:
CHECKED BY: WSM
DATE: 05/02/2014
REVISIONS:

SCALE: 1"=60'
SHEET NO.
1 OF 1
JOB NO.
S1400022.00



SCALE: 1" = 4000 FT.

SITE



CITY OF HARRISONBURG

Rezoning

West of N. Main St. and South of Mt. Clinton Pike

(Northside, LLC)

R-2 to B-2C

Tax Map Parcels: 42-B-8B, 8C, 32, 33, 34, 35, & 35A.

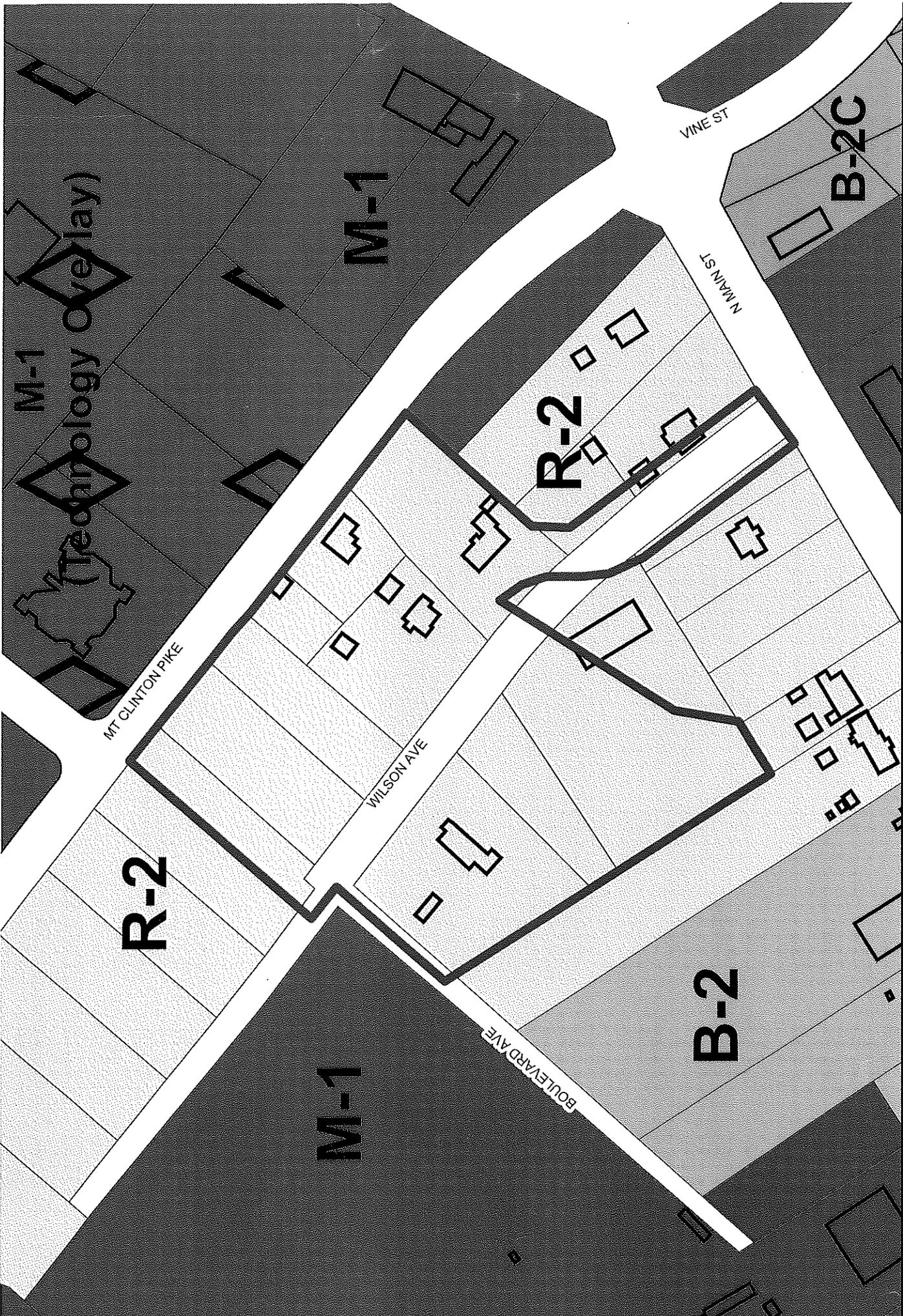
Portions of 42-B-8, 8A, 9, 9A, & 36 and 44-A-31, along

with portions of Wilson Avenue and Boulevard

Avenue found on tax maps 42 and 44.

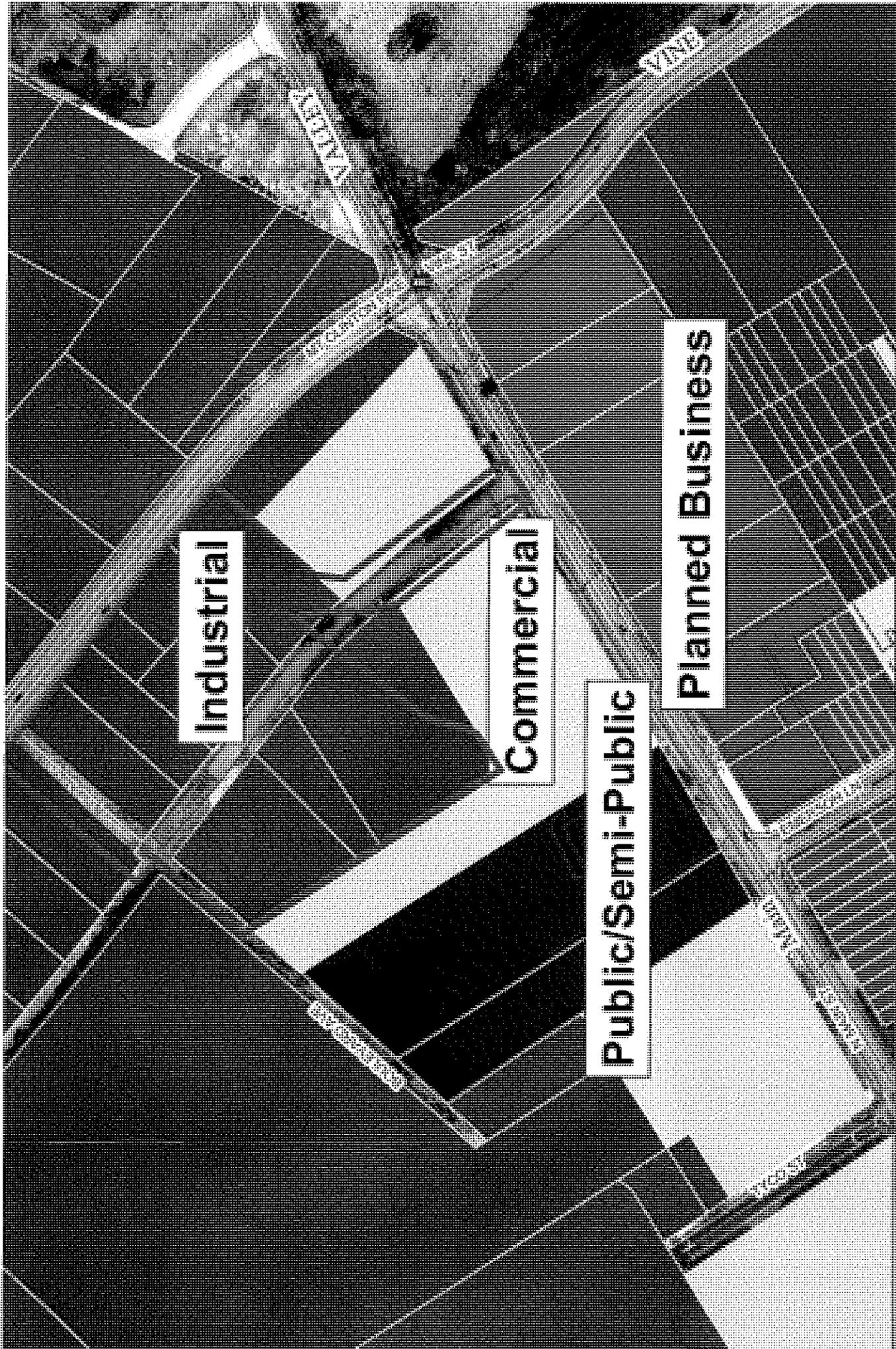
6.69 +/- acres

LOCATION MAP



**West of North Main Street and South of Mt. Clinton Pike
Northside, LLC - R-2 to B-2C**





**Comprehensive Plan Land Use Guide Designations
West of North Main St. and South of Mt. Clinton Pike
Northside LLC, Rezoning Request R-2 to B-2C**



City of Harrisonburg, Virginia

DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT

STAFF REPORT

June 11, 2014

REZONING – WEST OF NORTH MAIN STREET & SOUTH OF MT CLINTON PIKE

GENERAL INFORMATION

- Applicant:** Northside, LLC and Joseph and Linda Moore
- Tax Map:** 42-B-8B, 8C, 32, 33, 34, 35, & 35A, and portions of 42-B-8, 8A, 9, 9A, & 36, along with portions of Wilson Avenue and Boulevard Avenue found on tax maps 42 and 44.
- Acreage:** 6.69 +/- acres
- Location:** West Side of North Main Street and South Side of Mt. Clinton Pike
- Request:** Public hearing to consider a request to rezone seven parcels and portions of five parcels and portions of Wilson Avenue and Boulevard Avenue from R-2 Residential District to B-2C, General Business District Conditional.

LAND USE, ZONING, AND SITE CHARACTERISTICS

The Comprehensive Plan designates the majority of this area as General Industrial with small portions designated as Commercial. The General Industrial designation states that these areas are composed of land and structures used for light and general manufacturing, wholesaling, warehousing, high-technology, research and development and related activities. The Commercial designation states that these areas include uses for retail, office, wholesale, or service functions. These areas are generally found along the City's major travel corridors and in the Central Business District of the City

The following land uses are located on and adjacent to the property:

- Site:** Single Family homes, undeveloped lots, and portions of developed Wilson Avenue, and undeveloped Wilson Avenue and Boulevard Avenue, zoned R-2
- North:** Undeveloped parcels, zoned R-2, and across Mt. Clinton Pike, technology industrial park uses and undeveloped lots, zoned M-1 with the Technology Overlay
- East:** Undeveloped parcels, zoned R-2 and M-1, and across Mt. Clinton Pike parcels containing Harrisonburg Motor Express, zoned M-1
- South:** Single family homes, zoned R-2, and across North Main Street, industrial uses, zoned M-1
- West:** Single family home lot, zoned R-2, and property owned by GSW Investors with operations of Rockingham Construction and Special Fleet Service, Inc.

EVALUATION

The applicants are requesting to rezone 6.69 +/- acres of property made up from seven parcels, portions of five other parcels, and a majority of the sections of Wilson Avenue and Boulevard Avenue that they requested for closure last month, all from R-2, Residential District to B-2C, General Business District Conditional. (As a reminder, the street closing request was recommended for approval by staff and also supported by Planning Commission with a 6-0 vote on May 14th. The public hearing for the street closing is scheduled at City Council on June 10th.) Along with this rezoning, the applicants are also requesting preliminarily plat approval for the subject property, the details of which are explained in a separate staff report. The street right-of-way (ROW) closure, the preliminary plat, and the rezoning are all steps initiated by the applicants so that they may sell the property to an interested commercial developer to build a grocery store.

The applicants' engineer submitted three layout sheets to demonstrate different details of the request. Sheet 1 illustrates the conceptual layout for the grocery store, Sheet EX-3 demonstrates the exact area of the applicants' property they are requesting be rezoned, and Exhibit A illustrates locations of cross access easements that would be provided for some of the neighboring properties.

In this rezoning, the applicants have proffered the following:

1. Cross access easements with adjoining properties shall be provided as generally shown on Exhibit A. The access road from North Main Street shall be a private road and the access road from Mt. Clinton Pike shall be a public street.
2. The following uses shall be prohibited on this property.
 - a. Vehicle, recreation equipment, or trailer sales served by a permanent building facility unless clearly incidental to an existing building. Vehicle excludes over the road tractors, their trailers, heavy equipment, manufactured homes, industrialized buildings, and agricultural equipment. No vehicle salvage, storage of inoperable vehicles, or sale of junk is allowed.
 - b. Repair of vehicles, recreation equipment, or trailers with all activities and storage of inoperable vehicles completely enclosed within a permitted structure. Vehicle excludes over the road tractors, their trailers, heavy equipment, manufactured homes, industrialized buildings, and agricultural equipment.
 - c. Tractor Trailer fueling stations selling diesel shall be prohibited on this property.
3. A shared use path will be constructed along Mt. Clinton Pike across tax map parcel 44-A-30 to the western property line of tax map parcel 42-B-11 (Northside, LLC). Adequate right-of-way will be dedicated, or easements provided, to accommodate the shared use path including two-feet beyond the width of the path. The path will be constructed in accordance with the AASHTO standards current at the time that the design of the facilities was performed. The shared use path will consist of a 10-foot wide asphalt surface with a 5-foot wide grass strip between the path and adjacent back of curb.
4. A left turn lane with 150 feet of storage and 100 foot taper turning into the new public road extension of Technology Drive along with a left turn lane of 100 feet and 100 foot taper turning into Technology Drive will be constructed on Mt. Clinton Pike.

5. A signal design will be provided for the intersection of Mt. Clinton Pike and Technology Drive and the developer will install all necessary conduits and all junction boxes during the road construction.
6. The extension of Technology Drive will be constructed from Mt. Clinton Pike to the existing boundary line of the remaining right-of-way portion of Wilson Avenue.
7. For any required sidewalk and curb and gutter construction along public streets, the stormwater will be managed with the proposed development.

Take note that the conceptual grocery store layout as shown on Sheet 1 is not proffered. However, Sheet 1 generally depicts the proffered street improvements that would be made along Mt. Clinton Pike and North Main Street.

The submitted rezoning request triggered the need for the applicant to perform a traffic impact analysis (TIA), which has already been reviewed. It should be understood that, along with other types of development applications, the Zoning Ordinance may require applicants of rezonings to perform a TIA if the rezoned property could generate 100 vehicle trips in the peak hour. When TIAs are required at the rezoning phase, they must be submitted and reviewed by the City before the rezoning application is accepted. Rarely are applicants required to perform a TIA at the time of rezoning because applicants often proffer the uses or other specifics of a project so that the TIA threshold is not triggered. As noted, for this request the threshold was triggered and the applicants had to complete a TIA.

After completing the TIA, the Department of Public Works, the Virginia Department of Transportation (VDOT), and the applicants' engineer determined that a depth of about 30 feet of public street ROW dedication would be needed along the subject property's entire Mt. Clinton Pike street frontage to accommodate the public street improvements required by the planned commercial development. VDOT was involved in the TIA review because the site is located within 3,000 feet of a State maintained road. Such a TIA is generally known as a Chapter 527 review, which is required by State Code. The required street improvements are proffered details as listed above. The needed ROW dedication is also demonstrated on the subject property's submitted preliminary plat.

The majority of the subject property is designated General Industrial by the Comprehensive Plan's Land Use Guide. However, small portions of the property are designated Commercial. The areas designated Commercial are the parts of the existing parcels adjacent to Wilson Avenue—along the sides of the conceptual plan's private road that extends from North Main Street to the planned development area. The subject property is bordered by more land designated General Industrial along a portion of its western boundary. Property across Mt. Clinton Pike is also designated General Industrial. However, aside from a small adjacent area at the corner of North Main Street and Mt. Clinton Pike, the subject property is bordered on the east, south and a portion of its western boundary by land designated as Commercial. The Commercial designation was placed on lots fronting North Main Street. Except the very corner parcel (tax map 42-B-I1) and the two properties where the Community Services Board operates, all properties having frontage along North Main Street are designated as Commercial. This commercially designated corridor stretches south to North Main Street's intersection with Washington Street, where the Comprehensive Plan's Land Use Guide changes to Mixed Use Development Areas—a Land Use Guide designation often associated with B-1 zoning.

Although the property is primarily designated for General Industrial use it is in an area of transition between General Industrial and Commercial use. In fact, the Comprehensive Plan land use guide uses one of the western boundaries of the subject property as the split between future General Industrial and Commercial use. Another factor in staff's favorable recommendation is transportation and how traffic should efficiently and effectively move through this area and the North Main Street/Mt. Clinton Pike intersection. Although the City desires to have commercial properties fronting North Main Street, entrances to such properties should be located as far from the North Main Street/ Mt. Clinton Pike intersection as reasonably possible. The proposed rezoning provides an opportunity to minimize entrances close to this intersection by providing access easements to the lots located closest to the corner. The associated subdivision and street closures also include the extension of Technology Drive, providing the opportunity for connectivity with Mount Clinton Pike to undeveloped land north and west of the rezoning.

Taking into consideration that the Comprehensive Plan designates much of the adjacent land for commercial use and also given the larger transportation/traffic picture, staff feels it is appropriate to recommend approval of this rezoning.

Although recommending in favor of the requested rezoning, staff does not believe this sets a precedent to look favorably upon further B-2 rezoning requests along the Mt. Clinton Pike corridor. At this time, staff believes the appropriate limit to the B-2 zoning district is at the planned extension of Technology Drive.

MVG – Mt. Clinton Pike Re-Zoning Narrative

The intent of the proposed rezoning request is to provide the uses as exhibited on the Concept Plan provided by Balzer and Associates Inc. titled MVG Development dated 5/12/2014. The proposed grocery and gas station use requires B-2 zoning. It is understood that based on the comprehensive plan the remaining parcels along North Main St will also be a B-2 use and property to the west of the site will be industrial M-1 uses. Due to this, there will be several provisions that will need to be considered and accounted for in the development of this initial property. We have outlined several items starting with transportation, then utilities and last stormwater to describe the intent of the development strategy.

Based on future growth and access management guidelines, a private road is proposed through the future B-2 uses onto North Main St. which will have access easements granting these adjoining properties the right to access and utilize this road. A public road to the west that is opposite Technology Drive on Mt Clinton Pike shall also be constructed providing access to these adjoining properties. These entrances will be built to accommodate one inbound lane as well as two outbound lanes. Through the traffic study, we have identified the improvements required to make the access points to the site safe. A shared use path has also been identified as another improvement that will be constructed from the new public road to North Main Street along Mt. Clinton Pike. The access to the many homes that exist today will be maintained until such time comes that the development requires them to be removed.

Currently there are many properties that make up this tract that have public water and sewer available. Through the redevelopment of these parcels many of the existing utility lines will need to be relocated but the design will need to honor existing systems that will not be adjusted as well as provide the same level of service to the remaining parcels as they exist today. These systems will be coordinated through the City departments.

Stormwater is always a concern. A development plan accounting for future growth will be provided. The concept plan outlines an area where the majority of the stormwater generated with this development will be managed. Currently, Mt. Clinton Pike as well as the parcels along North Main St. drain through this site. Provision will need to be made for extension to existing systems and new systems provided to allow for future connections through the site. Stormwater will be managed according to state and local regulations.

While many of these improvements will be managed and engineered through the development phase, it is important to have an understanding of the challenges and provision required. Close coordination with the development group, the current property owners that are retaining many parcels around that development, and the City staff will be required. Many different plats will be needed to account for the vacation of Wilson Avenue and Boulevard Avenue as well as a consolidation plat of remaining parcels. The proposed development plan does provide the infrastructure to allow for the future growth and is in accordance with best management design practices.

MVG – Mt. Clinton Pike Re-Zoning Proffers

I (we) hereby proffer that the development of the subject property on this application shall be in strict accordance with the conditions set forth in this submission.

1. Cross access easements with adjoining properties shall be provided as generally shown on Exhibit A. The access road from North Main Street shall be a private road and the access road from Mt. Clinton Pike shall be a public road.
2. The following uses shall be prohibited on this property:
 - a. Vehicle, recreation equipment, or trailer sales served by a permanent building facility unless clearly incidental to an existing building. Vehicle excludes over the road tractors, their trailers, heavy equipment, manufactured homes, industrialized buildings, and agricultural equipment. No vehicle salvage, storage of inoperable vehicles, or sale of junk is allowed.
 - b. Repair of vehicles, recreation equipment, or trailers with all activities and storage of inoperable vehicles completely enclosed within a permitted structure. Vehicle excludes over the road tractors, their trailers, heavy equipment, manufactured homes, industrialized buildings, and agricultural equipment.
 - c. Tractor Trailer fueling stations selling diesel shall be prohibited on this property.
3. A shared use path will be constructed along Mt. Clinton Pike across tax map parcel 44-A-30 to the western property line of tax map parcel 42-B-11 (Northside, LLC). Adequate right-of-way will be dedicated, or easements provided, to accommodate the shared use path including two-feet beyond the width of the path. The path will be constructed in accordance with the AASHTO standards current at the time that the design of the facilities was performed. The shared use path will consist of a 10-foot wide asphalt surface with a 5-foot wide grass strip between the path and adjacent back of curb.
4. A left turn lane with 150 ft of storage and 100' taper turning into the new public road extension of Technology Drive along with a left turn lane of 100 ft and 100' taper turning into Technology Drive will be constructed on Mt Clinton Pike.
5. A signal design will be provided for the intersection of Mt Clinton Pike and Technology Drive and the developer will install all necessary conduits and all junction boxes during the road construction.
6. The extension of Technology Drive will be constructed from Mt Clinton Pike to the existing boundary line of the remaining right-of-way portion of Wilson Avenue.
7. For any required sidewalk and curb& gutter construction along public streets, the stormwater will be managed with the proposed development.

Date Application Received: 04-08-14 (PC Meeting/Agenda for June 2014)

Total Paid: \$585.00 AF

Application for Change of Zoning District City of Harrisonburg, Virginia

Section 1: Property Owner's Information

Name: Northside, LLC Attn: James Moore
Street Address: 9527 Centerville Road Email: jemjam1@aol.com
City/State/Zip: Bridgewater, Va 22812
Telephone (work): 540-828-7432 (home or cellular): 540-405-4495 (fax): _____

Section 2: Owner's Representative Information

Name: Balzer & Associates, Inc. Attn: William S. Moore
Street Address: 1561 Commerce Rd, Suite 401 Email: wmoore@balzer.cc
City/State/Zip: Verona, VA 24482
Telephone (work): 540-248-3220 (home or cellular): 540-294-0258 (fax): _____

Section 3: Description of Property

Location (street address): Mt. Clinton Pike & North Main Street (see attached exhibit)
Tax Map Number: Sheet: (various) Block: (various) Lot: (various) Total Land Area (acres or square feet): 7.0 acres *(6.69 AF)*
Existing Zoning District: R-2 ~~MM~~ AF Proposed Zoning District *: B-2C *(6.468 acres)*
Existing Comprehensive Plan Designation: Commercial & Industrial

**If applying for conditional rezoning, provide a letter stating proffers on separate sheet of paper*

Section 4: Application Fee

\$375.00 plus \$30.00 per acre, and if applicable, Fees for a Traffic Impact Analysis (TIA) Review (see below)

- (a) Would the development from this rezoning require a Traffic Impact Analysis by VDOT?
Yes No

If yes, then fees must be made payable to VDOT to cover costs associated with the TIA review.

PLEASE NOTE – If a TIA is required, this application shall not be considered accepted until the TIA has been reviewed.

- (b) Would the development from this rezoning require a Traffic Impact Analysis review by the City?
Yes No

If yes, then an additional \$1,000.00 must be made payable to the City to cover costs associated with the TIA review.

PLEASE NOTE – If a TIA is required, this application shall not be considered accepted until the TIA has been reviewed.

Section 5: Names and Addresses of Adjacent Property Owners (Use separate sheet for additional names)

North: (see attached)
East: (see attached)
South: (see attached)
West: (see attached)

Section 6: Certification

I certify that the information contained herein is true and accurate. Signature: James E Moore *MANAGER*
Property Owner 4-8-2014

See Back for Items Required for Submission

GROCERY TENANT
MT CLINTON PIKE

SITE #XX - VERSION 1.0

CITY OF HARRISONBURG, VIRGINIA

DRAWN BY ATE

DESIGNED BY ERB

CHECKED BY SMH

DATE 06-03-2014

SCALE 1" = 50'

REVISIONS:
6-5-14
6-6-14

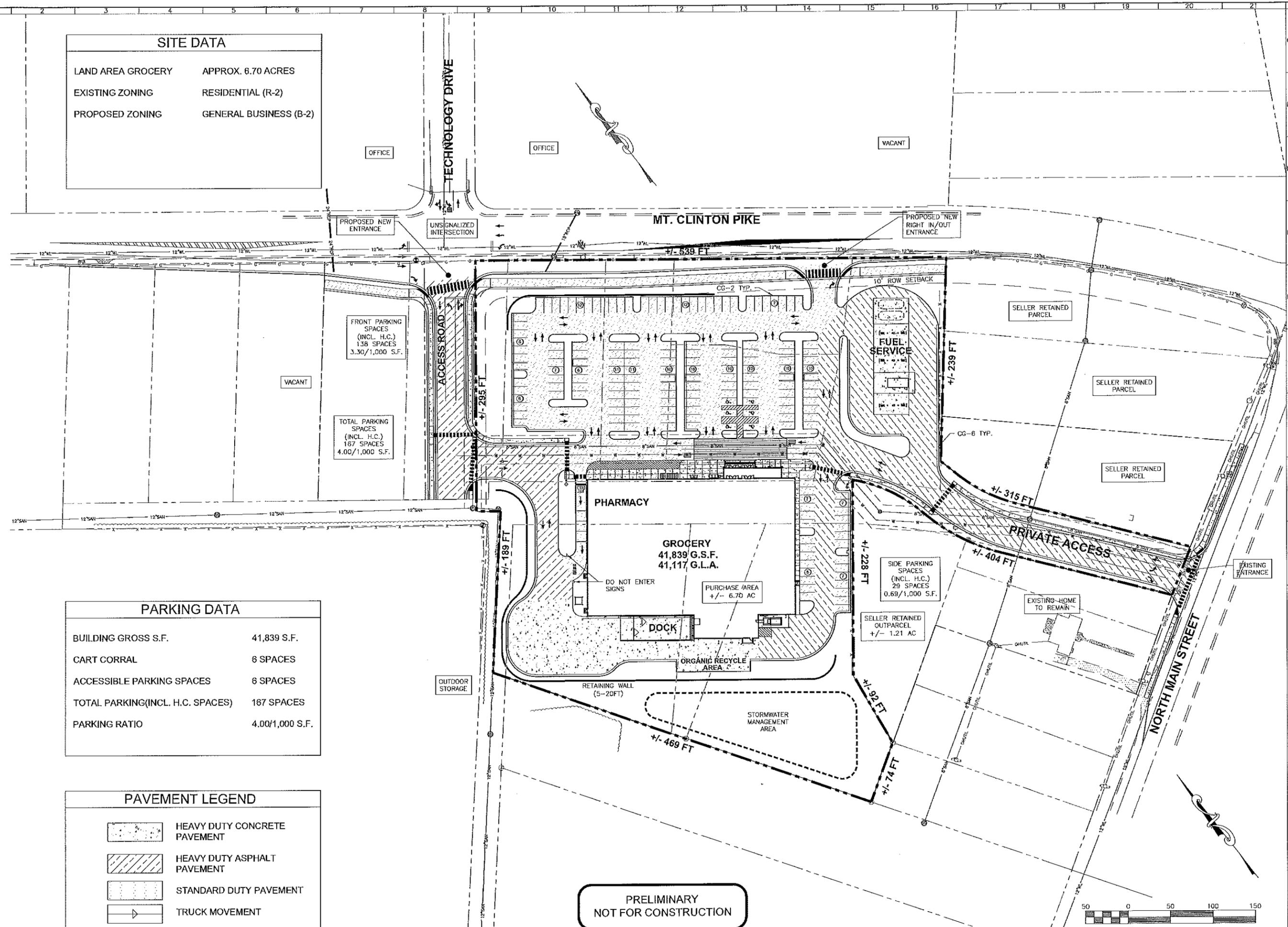
SHEET NO.

SITE DATA	
LAND AREA GROCERY	APPROX. 6.70 ACRES
EXISTING ZONING	RESIDENTIAL (R-2)
PROPOSED ZONING	GENERAL BUSINESS (B-2)

PARKING DATA	
BUILDING GROSS S.F.	41,839 S.F.
CART CORRAL	6 SPACES
ACCESSIBLE PARKING SPACES	6 SPACES
TOTAL PARKING (INCL. H.C. SPACES)	167 SPACES
PARKING RATIO	4.00/1,000 S.F.

PAVEMENT LEGEND	
	HEAVY DUTY CONCRETE PAVEMENT
	HEAVY DUTY ASPHALT PAVEMENT
	STANDARD DUTY PAVEMENT
	TRUCK MOVEMENT

PRELIMINARY
NOT FOR CONSTRUCTION





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LANDSCAPE ARCHITECTURE
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ARCHITECTURE
STRUCTURAL ENGINEERING

Balzer and Associates, Inc.
15871 City View Drive
Suite 200
Midlothian, VA 23113
804-794-0571
FAX 804-794-2635

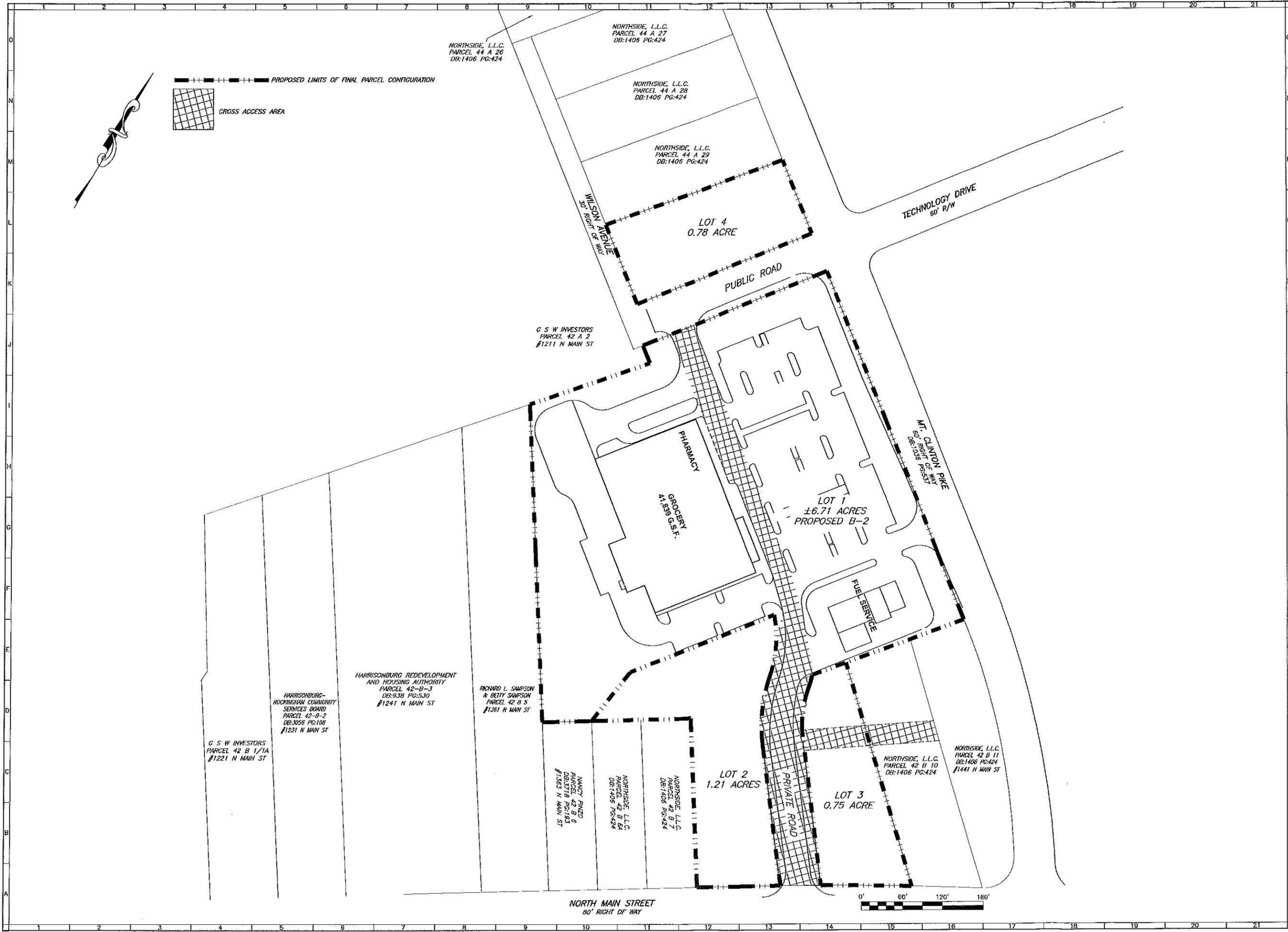
EXHIBIT SHOWING
CROSS ACCESS AREA
CITY OF HARRISONBURG, VIRGINIA

DRAWN BY JAP
DESIGNED BY
CHECKED BY WSM
DATE 05/01/2014
SCALE 1"=60'

REVISIONS:
06/02/2014

SHEET NO.
Exhibit A

JOB NO. S1400022



Downtown Streetscape Plan

for

Harrisonburg, VA

Revised

May, 2014



ACKNOWLEDGMENTS

The Downtown Streetscape Plan was prepared by City staff with the support of the Downtown Streetscape Plan Advisory Committee, the Harrisonburg Downtown Renaissance (HDR) Landscape Committee, McCormick Taylor, Inc., John Sease of Sease & Associates, P.C., and the citizens of Harrisonburg.

ADVISORY COMMITTEE MEMBERS

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Ande Banks, City Manager's Office
Eddie Bumbaugh, Harrisonburg Downtown Renaissance
Thanh Dang, Department of Public Works
Adam Fletcher, Department of Planning & Community Development
Dave Miller, Dave's Taverna
Brad Reed, Department of Public Works
Brian Shull, Department of Economic Development
Reggie Smith, Department of Public Transportation
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Jesse Bergey – Fine Earth
Randy Buie – Shenandoah Historic Restoration Alliance
Eddie Bumbaugh – Harrisonburg Downtown Renaissance
Suzi Carter – Northend Greenway
Charles Hendricks – The Gaines Group, PLC Architecture + Design
Nate Miller – Valley 25X25, Simply Sustainable Landscape Designs
Matt Robertson – The Stratford Companies
Karl Shank – The Natural Garden
Adam Steiniger – Eugene Stoltzfus Architects
Dave Wiens – Harrisonburg City Council (former)
Mike Zook – Great Outdoors Landscaping

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Charles Chenault, Vice-Mayor
Richard Baugh
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Abe Shearer

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I. INTRODUCTION

BACKGROUND

This Downtown Streetscape Plan builds upon the work already performed on the Harrisonburg Downtown Streetscape Plan ("Streetscape Plan") developed by Frazier Associates in 2003-2004 and adopted in 2005 by City Council. That Streetscape Plan was developed to provide a coherent vision to help guide city decision making in hardscape material choices, conceptual designs and landscaping materials. The previous downtown streetscape work undertaken in the 1970s was outdated and in need of significant repair by the early 2000s. Since 2005, the City has implemented many features of the Streetscape Plan, including streetscape work on the east side of South Main Street between Bruce Street and Elizabeth Street; sidewalk enhancements on West Bruce Street between South Main Street and South Liberty Street; West Market Street from High Street to Liberty Street; and East Water Street from South Main Street to South Federal Street. The City has also placed utilities underground along the Water Street corridor between Mason and Liberty Streets.

Increasing investment interest in the downtown area is evidenced by a number of major development projects since 2007, including: the renovation of the Wetsel Seed Building on Noll Drive, which was converted into a restaurant and residential housing known as "City Exchange"; new construction known as "Urban Exchange" on East Market Street just east of its intersection with Mason Street; the restoration of a former sewing factory to residential units along West Rock Street known as "Sancar Flats at West Rock"; the renovation of the Wetsel Seed Building on North Liberty Street now housing the "Wetsel Complex" as offices, retail storefronts, and a restaurant; the expansion of Rosetta Stone with the restoration of the Wetsel Seed Warehouse and the Old Creamery Building once occupied by the Harrisonburg Police Department; the construction of a mixed use residential and commercial complex along North Mason Street called the "Colonnades at Rocktown;"; and the renovation of the former Cassco Ice industrial facility along South Liberty Street and West Bruce Street now known as the "Ice House," which currently is occupied by employees of James Madison University and remains under construction for a mixture of commercial and residential uses. The City also undertook the implementation of a citywide "wayfinding" sign program to assist visitors as they seek destinations in downtown among other landmark destinations.



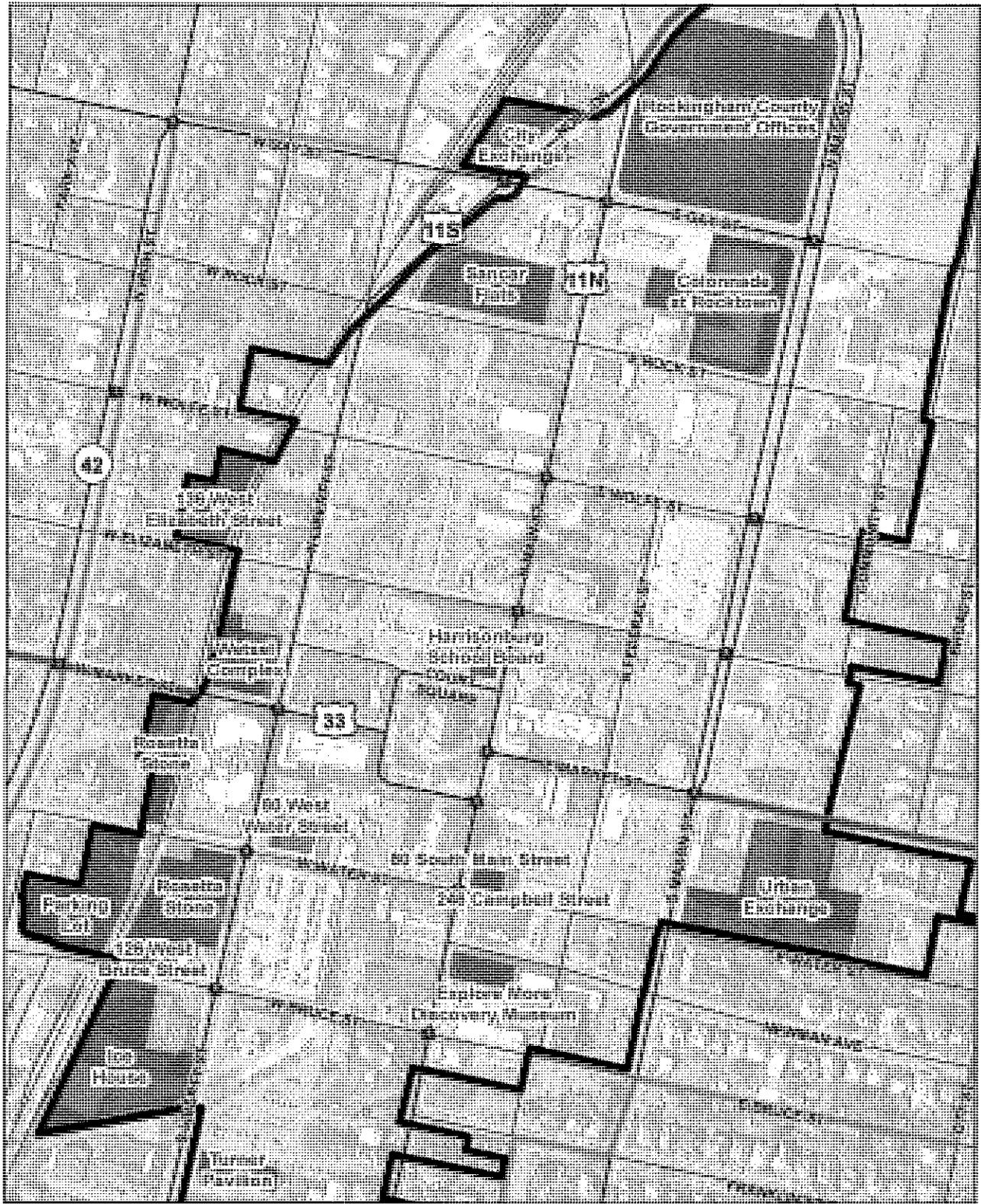
"Wayfinding" sign on Main Street

The noteworthy redevelopment of downtown and the emergence of new plans signify an important reality; a comprehensive plan is needed to communicate the collective vision for the historic and cultural center of the Harrisonburg-Rockingham community. Implementation of the plan requires that scarce public resources be spent wisely. This plan expands the work performed in the 2005 Streetscape Plan and supersedes it. This plan communicates a clear and concise vision for the Central Business District and the immediate surrounding areas, listed here as the "Transition Area". With the expansion of development opportunities in the Central Business District, it is expected that the redevelopment and revitalization of the Transition Area will unfold in a matter of years.

Additionally, several historic structures have been demolished in Harrisonburg's Historic Districts since 2005. Harrisonburg differs from many other Virginia communities that have fostered substantial preservation and revitalization programs, which often focused on retaining historic structures. While the City does have established historic districts, it is one of the few in Virginia that does not have any design standards accompanying the designation. In that respect, revitalization has taken a different approach for this community. This plan does not attempt to establish physical design standards for private structures, though the exploration of this option is a strategy (15.1.2) in the City's 2011 Comprehensive Plan.

Without a comprehensive downtown streetscape plan that coordinates public and private efforts, public interest may be underserved. Without planning and forethought given to future needs, growth will be stymied instead of nurtured. At the same time, it should be recognized that this is a plan, and one which will require revision and realignment from time to time, as economic realities, private investment and public sentiment changes.

One of the unique aspects of Harrisonburg's ongoing downtown development is the decision to preserve the rights of property owners. This plan is consistent with this philosophy, and is not intended to establish an architectural review board or to set forth design standards that property owners are beholden to. This is a plan for an investment in public infrastructure downtown. It sets forth a clear vision so that expenditures can be prioritized in future years and also offers a plan to guide property owner and developer interests in coordinating their developments with the public streetscape.



Major Development Projects (2007-2014)

Downtown Streetscape Plan



There are multiple planning documents that serve to govern different aspects of public infrastructure in and surrounding the immediate Central Business District. Some of these plans are conceptual in nature and have not been officially adopted by City Council as “plans” that could govern public infrastructure investment and private development partnership. Some of the requirements and recommendations in these plans refer to the City-at-large and do not address the specific needs and conditions in downtown.

- Harrisonburg Downtown Streetscape Plan, 2005 - Created by the Downtown Revitalization and Streetscape Advisory Committee with guidance from Frazier Associates, this document puts forth a phased improvement plan for the downtown area. Improvements to features such as sidewalks, street lighting, and benches are outlined along with strategies for parking, pedestrian safety, and wayfinding. The plan also establishes sub-districts within downtown based on varied defining characteristics. The 2005 plan was adopted by City Council and will be superseded by this new plan.
- Comprehensive Plan - This document presents the vision of the kind of community the City would like to be and goals to achieve the vision. The downtown area is referred to throughout the Comprehensive Plan (Chapter 8, Historic Resources; Chapter 11, Transportation; Chapter 13, Economic Development & Tourism; and Chapter 14, Revitalization). The Master Transportation Plan is a component of the Comprehensive Plan and establishes the city's long-range transportation policies and street improvement projects. The Master Transportation Plan includes the Street Improvement Plan, the Bicycle & Pedestrian Plan, and the Transit Development Plan. This plan has been adopted by City Council and is reviewed every 5 years.
- Bicycle & Pedestrian Plan - This document details existing policies and facilities for bicycle and pedestrian traffic in Harrisonburg and puts forth priority infrastructure improvement projects and the means by which they are to be achieved. This plan has been adopted by City Council and is updated every 5 years.
- Transit Development Plan - A short-range plan that outlines the services that the Harrisonburg Department of Public Transportation intends to implement. The plan estimates what resources will be needed and what funding opportunities are likely to be available to achieve these goals. This plan has been adopted by City Council and is updated every 6 years.
- Capital Improvement Plan - A product of planning work completed by city departments, this document lays out public improvement projects for which departments wish to have funds allocated during the next 5 years. This plan is reviewed and adopted by City Council each year.
- A Parking Plan for Downtown Harrisonburg - Completed in 2009 as the successor of many downtown parking plans, this document identifies downtown parking needs, opportunities for new parking infrastructure, and financing opportunities for them. This plan has not been adopted by City Council.
- Parking Study by JMU Master of Public Administration Team - This study was presented to City Council in October 2013 and provides downtown parking recommendations based upon detailed user survey data. This study has not been adopted by City Council.

- Urban Values & Vision for Downtown Harrisonburg by Eugene Stoltzfus Architects – Architect Eugene Stoltzfus’ presentation and supplemental book portray a vision for the downtown area by suggesting ways to make downtown a more attractive and pedestrian friendly location. This document was commissioned by the Economic Development Committee of Harrisonburg Downtown Renaissance and has not been adopted by City Council.

There are three regulatory documents that govern development and infrastructure in the City-at-large and are relative to efforts set forth in this plan:

- Design & Construction Standards Manual (DCSM) – This manual defines guidelines and standards for public facilities and private site features constructed in the city and has been adopted by City Council.
- Zoning Code – The City’s zoning code defines the B-1 Central Business District, which is generally identified as the downtown area. This code has been adopted by City Council.
- Subdivision Ordinance – The City’s subdivision ordinance enforces provisions for the development of streets within and contiguous to any subdivision to ensure the goals of the Comprehensive Plan and other applicable city plans are met. This ordinance has been adopted by City Council.

There are several issues regarding the downtown that are not addressed by existing plans, creating difficulties for city government. Such issues include:

- how to integrate regular and routine maintenance projects to accommodate a collective vision compatible with the many viewpoints in the downtown area;
- implementation of the hardscape components of the city’s Streetscape Plan and that components of it are being accomplished in a piecemeal fashion and may be moving ahead without full consideration of other utilities and infrastructure needs;
- the lack of streetscape standards and plans to communicate to prospective developers desiring to locate within the B-1 zoning classification;
- how to provide additional parking when it is determined by City Council to be needed and where to strategically place it in an easily accessible location adjacent to transportation facilities that can accommodate future traffic volumes while coordinating it with desired streetscape improvements;
- how to manage solid waste issues for downtown businesses that require refuse collection outside of the city’s current business model.

This plan aims to address these issues by synthesizing and expanding upon existing documents to develop a long-term, achievable plan for enhancing the downtown streetscape. This includes providing a practical guide for public improvements on each downtown street and communicating these plans to the development community to share project costs amongst stakeholder groups.

This plan’s overall goal is: To present an easily communicable, comprehensive vision for the public streetscape in Downtown Harrisonburg that can be utilized by public and private agencies to further develop and sustain a vibrant downtown.

OBJECTIVES

1. Develop a comprehensive vision for the public right-of-way within the study area that defines public and private expectations for project elements during development and redevelopment of properties.
2. Provide a plan for safe and efficient pedestrian and bicycle accommodations that enhance the public streetscape and related public spaces.
3. Expand parking opportunities in coordination with streetscape enhancements to support future downtown business, residential housing, and visitor needs.
4. Enhance public transportation facilities to accommodate citizens and visitors in coordination with streetscape improvements.
5. Develop a plan for public services in the downtown area that addresses the changing needs of businesses.
6. Plan for necessary improvements to public and private utility infrastructure so that it does not impede or encumber streetscape improvements.
7. Partner with property owners and community stakeholders to provide opportunities for development or redevelopment of public and private downtown properties that could enhance the public streetscape.
8. Consider expansion of recreational and green space opportunities downtown that coordinate with public streetscape improvements.
9. Enhance the visual character of the downtown streetscape.

CHALLENGES

There are significant planning challenges that make Harrisonburg's downtown unique:

1. Downtown is bisected east to west by U.S. Route 33 (Market Street) and north to south by U.S. Route 11 (Main Street/Liberty Street/Noll Drive). As U.S. Route 11 serves as a secondary route to Interstate 81, frequent interstate traffic diversions push heavy traffic volumes through downtown.
2. Court Square acts as a quasi-traffic circle where tractor trailers and larger delivery vehicles have difficulty negotiating turns. Many other intersections create similar challenges for large vehicles due to their geometry.
3. Blacks Run traverses through downtown and, in many cases, has been "tunneled" underneath buildings, parking lots, and streets.
4. The Norfolk Southern Railway runs along the west side of downtown where most streets cross at-grade with the railroad tracks. The only grade separated crossing is a wooden bridge on West Water Street owned by Norfolk Southern, which has a weight restriction.

5. Harrisonburg's downtown was developed with narrow streets and, consequently, narrow sidewalks. While there are sidewalks throughout downtown that provide connectivity, in many cases sidewalks are narrow and encumbered with light poles, traffic signal poles, street furniture, and private advertising.



Narrow sidewalks on West Elizabeth Street

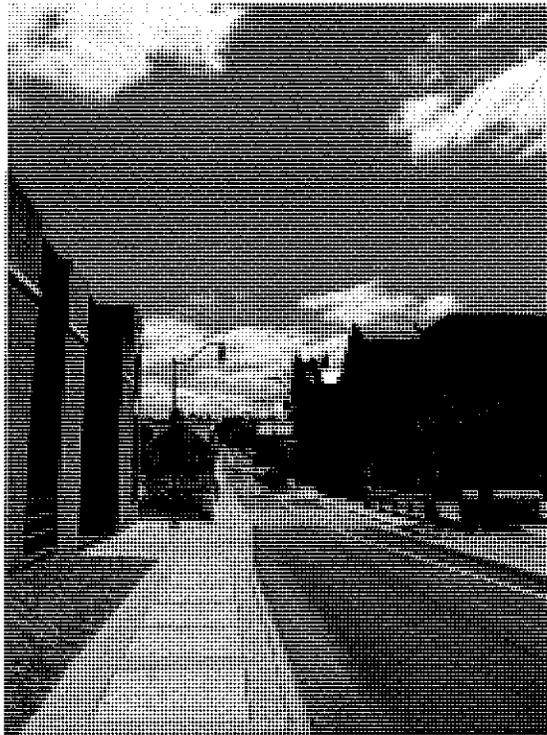
6. Downtown is served by some of the oldest public utility infrastructure in the City. Providing adequate water and sewer capacity is essential for continued economic growth and to maintain Needed Fire Flow (NFF) requirements for existing and new structures.
7. Public trash collection is provided between Monday and Friday by the City of Harrisonburg. Many restaurant establishments require additional trash collection resources that the City currently does not provide. There are currently few refuse storage areas in downtown and trash often clutters sidewalks.
8. Concepts for shared use paths (a.k.a. greenways and biking & walking trails) exist both north and south of downtown. Bringing a dedicated shared use path through downtown to connect these projects poses a challenge due to urban density, narrow streets, and the need to acquire additional property and/or convert on-street parking.
9. There is a desire for outdoor seating within the public sidewalk area at some restaurants downtown, which can be difficult to accommodate in many areas due to limited sidewalk space.

PLANNING PROCESS

This plan is overseen by the City of Harrisonburg Department of Public Works, Department of Planning & Community Development, Department of Economic Development, and the City Manager's Office. To establish a baseline format for a Downtown Streetscape Plan and understand challenges that may be involved in such an undertaking, Public Works staff researched the work completed in comparable localities. Through this process, it became apparent that localities with master plans for their downtown viewed the resulting document as an invaluable planning tool for achieving coordinated streetscape improvements, especially when care is taken to define a unified vision that is supported by the community-at-large.

Taking heed of these findings, the Downtown Streetscape Plan Advisory Committee was formed to represent the myriad stakeholders involved in this effort so that ideas can be vetted in a small group atmosphere during the various stages of the planning process.

This Committee had its first meeting in July 2010, wherein city staff gave an introduction to the Downtown Streetscape Plan concept, discussed the concept of a downtown merchant survey, and gathered input on how to redesign the West Bruce Street corridor between South Liberty Street and South Main Street when it was repaved in 2010.



West Bruce Street after 2010 improvements

To gather preliminary input from a broader stakeholder group, a survey of downtown businesses was conducted in September 2010 to gather suggestions and concerns related to public infrastructure. The results of the survey were used to formulate the scope of work for the plan. In January 2011, the Downtown Streetscape Plan Advisory Committee met again to discuss the survey results, share the scope of work, and to collaboratively perform a Strengths, Weaknesses, Opportunities, and Threats (SWOT) analysis for each of the subsections of the plan's study area. Building upon this assessment by the Committee, staff prepared its own analysis for each of the

sections of the plan. Public Works then hosted a series of meetings in February 2011 to discuss the needs of individual city departments.

Using the information gathered from these meetings, city staff developed the draft Downtown Streetscape Plan. This draft was shared with the Advisory Committee and the HDR Landscape Committee for input, updated, and then brought before the community in a public input meeting, titled "Sharing the Vision for Downtown". Comments collected from this meeting were reviewed and integrated into the final draft of the plan where appropriate.

In January 2014, Planning Commission held a public hearing on a final draft of the plan. Several comments were offered by the public and the Commission offered guidance on specifics of the plan. Staff re-evaluated the plan based upon this input and made revisions to the plan and refocused its content to more clearly specify the plan's vision and purpose.

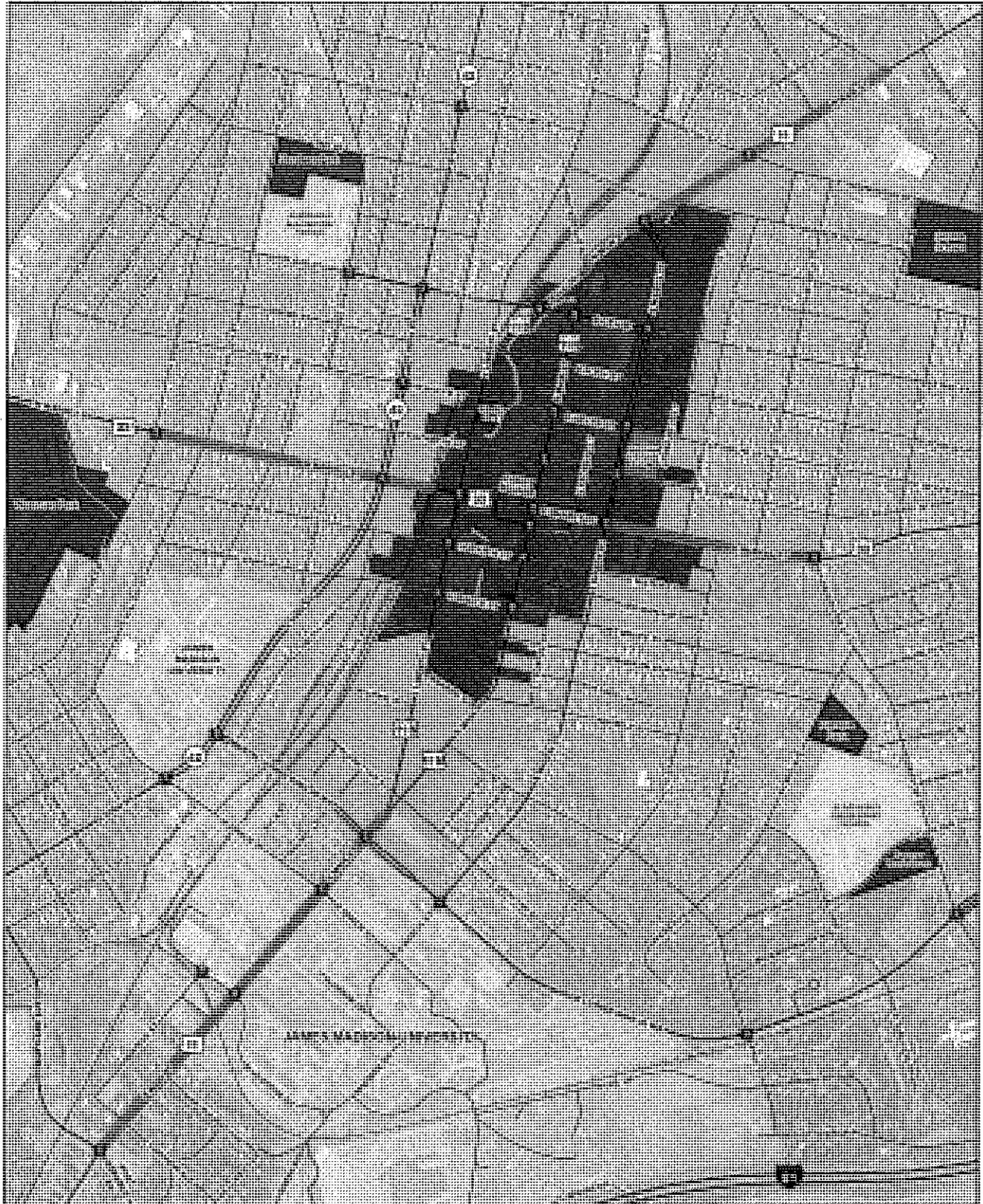
The final draft will be presented to the Harrisonburg Planning Commission to request their endorsement of the plan. When their endorsement has been received, staff will present the plan to City Council to request adoption.

Similar to the Comprehensive Plan, it is intended that the Downtown Streetscape Plan be a guide rather than a regulatory document, though it should be referenced in the City Design & Construction Standards Manual (DCSM) for use in defining public infrastructure improvements to be completed along the frontage of developing properties in the study area. The City should consider having the Comprehensive Plan reference the Downtown Streetscape Plan to identify it as the City's official vision and guide for public streetscape downtown.

STUDY AREA

The plan addresses three differentiated, but connected study areas:

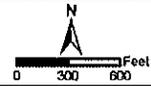
- Downtown Core - Commonly referred to as the Central Business District, this area comprises all parcels with B-1 zoning, a mixed use zoning classification. The boundaries of this area will change over time as parcels within the Downtown Transition Area are rezoned to B-1.
- Downtown Transition Area -The area adjacent to the Downtown Core for which public infrastructure improvements are recommended. This area is generally defined by the future mixed use development parcels identified in the 2011 Comprehensive Plan's Land Use section, but also includes other areas adjacent to the Downtown Core where improvements are recommended by this plan.
- Downtown Gateways - This area includes the portions of major street corridors (U.S. Route 11 and U.S. Route 33) that provide access to the Downtown Core and are recommended for streetscape improvements.



Study Areas
 Gateway
 Core
 Transition Area

Study Area Boundaries

Downtown Streetscape Plan



II. TRANSPORTATION

STREET NETWORK

The transportation network for the downtown and its immediate vicinity provides multi-modal and multi-purpose service for several divergent user groups, with typical users including area residents, downtown employees, college students, tourists, and commercial businesses. The widening breadth of dining, retail, and service establishments over the last decade has contributed to increasing occupancy levels and more complex travel patterns on downtown streets and sidewalks. Downtown streets need to have the capability of handling this expansion, while also fostering a pedestrian-scale atmosphere.

To meet the growing demand on downtown streets, creative planning and engineering must be used to maintain, if not improve, the level of service and safety of the public right-of-way. Due to space limitations and development density within the study area, this could involve the modification of prevailing travel patterns or the influencing of user perception to improve conditions. Multi-modal transportation improvements, including vehicle, pedestrian, bicycle, and public transportation enhancements may be needed to mitigate these challenges and create a welcoming environment for all travelers in the downtown area. This vision would align with the principles of the "complete streets" planning ideal to design streets to be operated by and enable safe access for all users, including pedestrians, bicyclists, motorists, and transit riders of all ages and abilities.

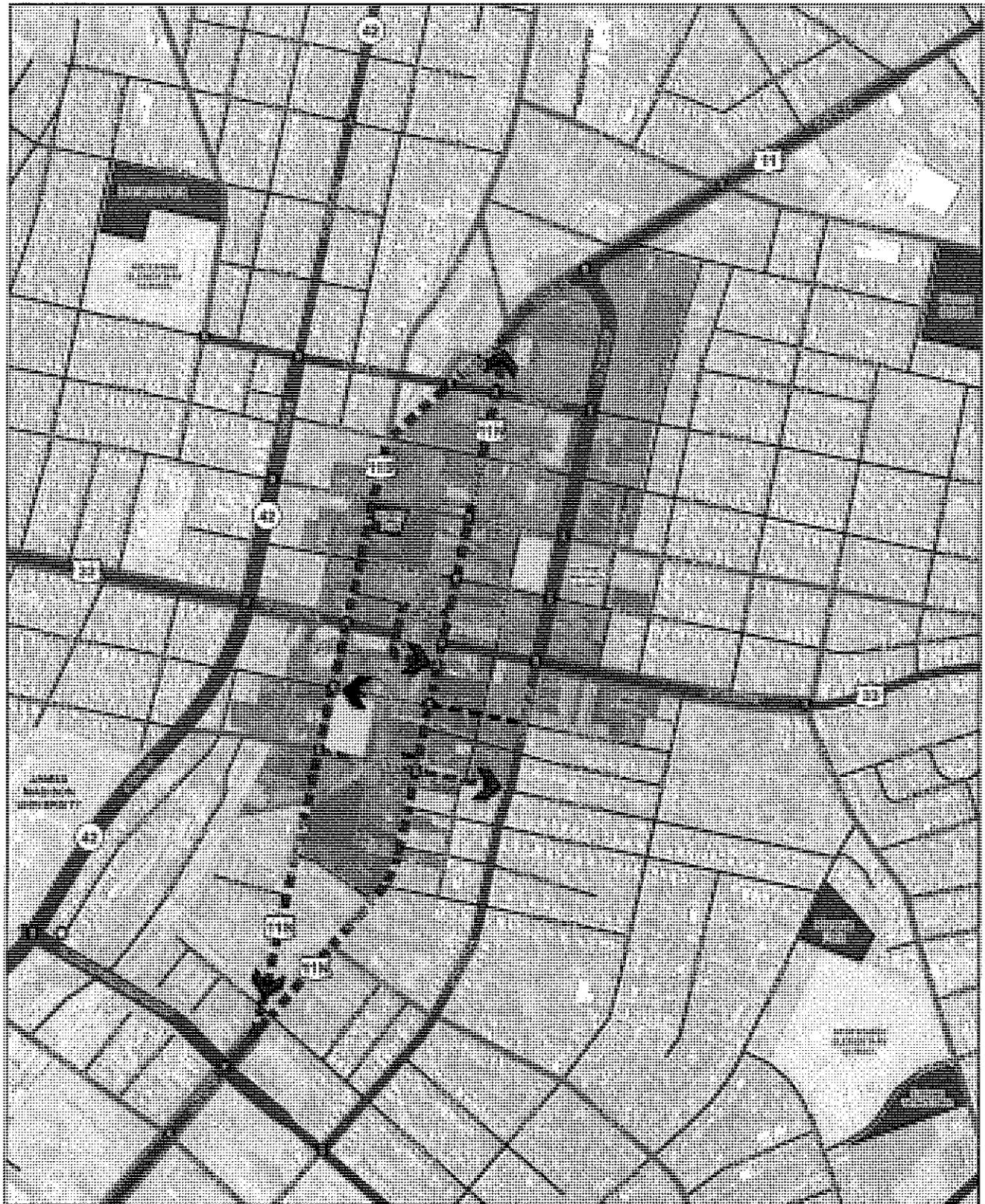
Most streets within the downtown area have sidewalks on both sides, but are limited in width. As pedestrian traffic increases, wider sidewalks are needed. The City's Bicycle & Pedestrian Plan recommends 10-15 foot wide sidewalks on routes with heavy pedestrian traffic. With streetscape improvement projects, there are opportunities on some streets to reduce vehicular lanes and widen sidewalks. This becomes especially important in areas where the effective width of the sidewalk is reduced by utility poles and street furniture. Many existing sidewalks are five feet wide, but fail to meet the four-foot minimum effective width called for by the Americans with Disabilities Act (ADA) guidelines due to utility poles in the middle of them.

Another key component in providing safe pedestrian travel is the provision of clear guidance for crossing signalized intersections. The Main Street & Court Square/Market Street intersection is the only location in the Downtown Core with pedestrian signals. Other intersections have marked crosswalks and it is the pedestrian's responsibility to cross with traffic, which becomes difficult on corners where visibility of the signal indication is obscured or signal heads are facing the opposite direction on one-way street sidewalks.

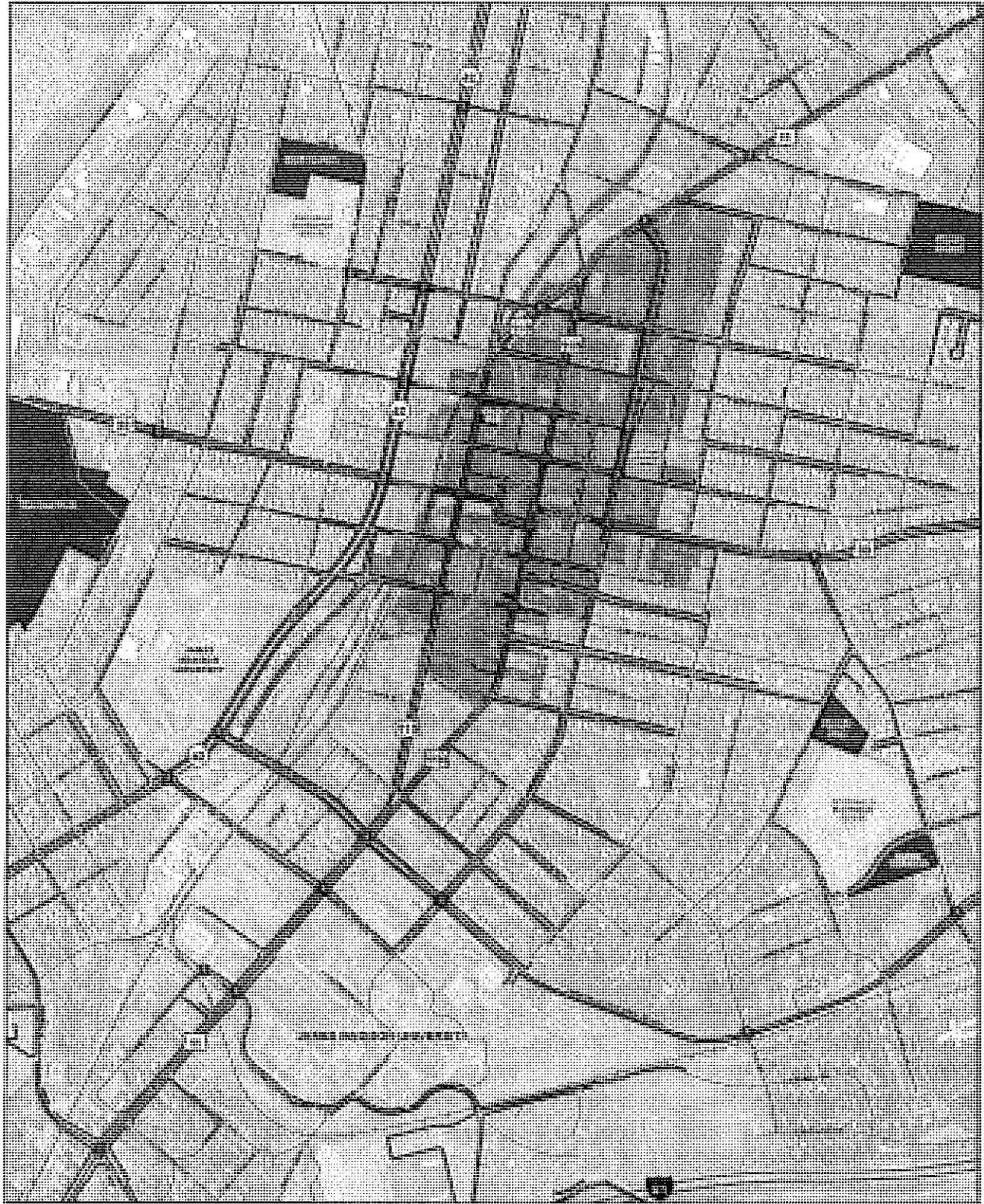
Recommendations related to the installation of new sidewalks, shared use paths, and signalized crosswalks are derived from the City's Comprehensive Plan and Bicycle & Pedestrian Plan. Priority locations recommended by this plan for sidewalk improvements, other pedestrian-related enhancements, and bicycle facilities can be found in the Pedestrian Facilities and Bicycle Facilities maps. Ideally sidewalks would be constructed on both sides of every street, but it is recognized that there are limitations in downtown Harrisonburg that will preclude this.

The number of bicyclists traveling on City streets has been steadily increasing in Harrisonburg. This has been most notable since 2005 when new bicycle facilities were constructed in the City, with growing interest in more active and healthy lifestyles, and interest in reducing environmental impacts by reduced dependence on cars. The inclusion of expanded bicycling facilities, both for travel and parking, is a key component of the Bicycle & Pedestrian Plan. The recommendations will

help guide bicycling-related recommendations downtown and are shown in the Bicycle Facilities Map.

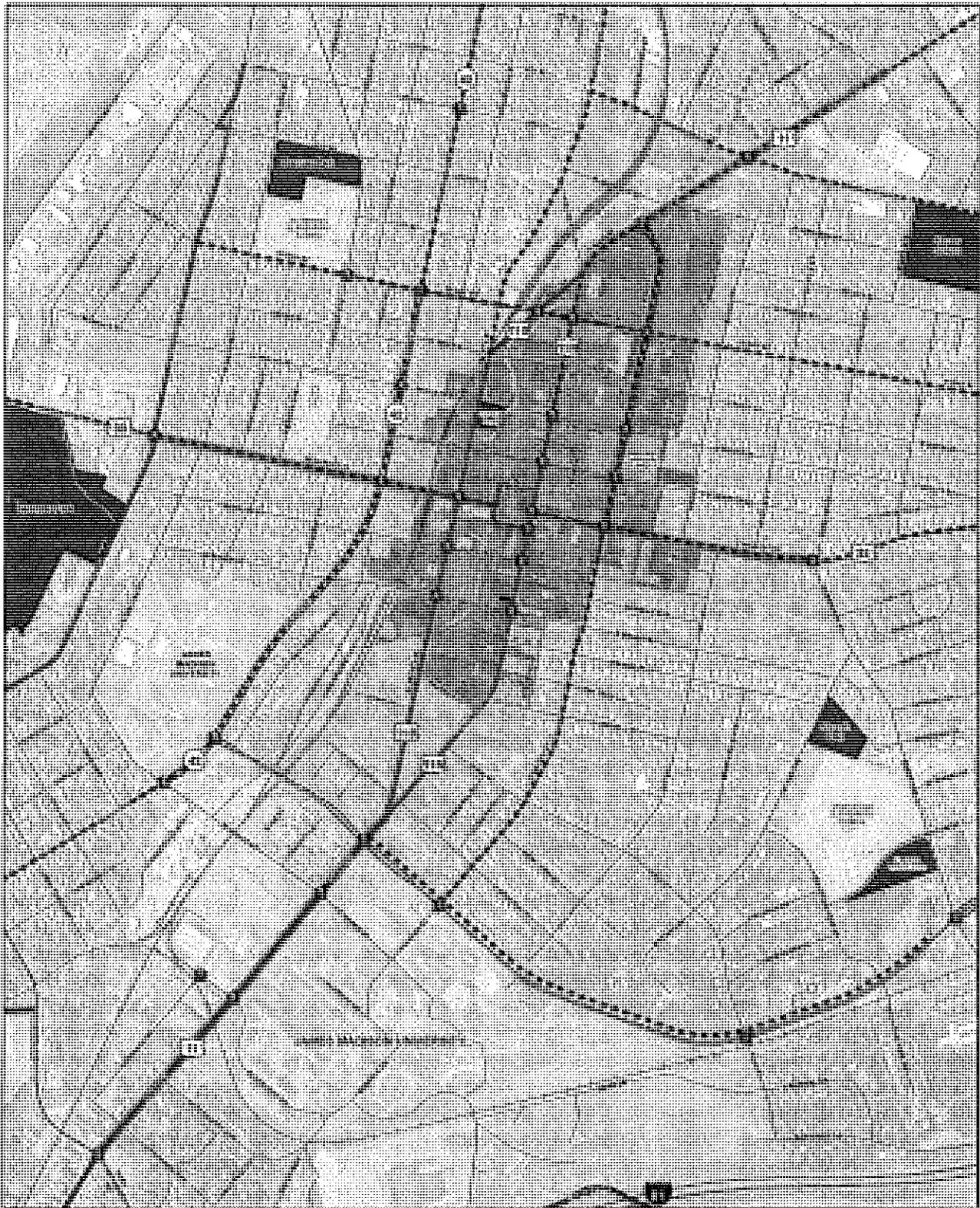


Transportation Overview
(Existing Conditions)
County Strategic Plan



	Pedestrian Facility		Street Name
	Street Name		City
	Street Name		Street Name
	Pedestrian Facility		Street Name

Pedestrian Facilities
Downtown Streetways Plan



Proposed Facilities	Existing Bicycle Facilities	Study Street
■ Shared lane	■ Shared lane	□ Study street
■ Shared lane with bike lane	■ Shared lane with bike lane	□ Bike lane
■ Bike lane	■ Bike lane	□ Bike boulevard
■ Bike boulevard	■ Bike boulevard	□ Bike share station
	■ Bike share station	

Bicycle Facilities
 Downtown Streetscape Plan



PARKING

Continued successful downtown revitalization efforts may lead to a need for expanded parking facilities. Since Harrisonburg is not an exceptionally urbanized community, residents and guests have an expectation that parking should be available within a very short distance of their destination. Since the majority of downtown businesses are currently concentrated between Bruce Street and Court Square, this high parking demand also overlaps with the area that is most heavily developed. Details as to the parking demand downtown can be found in the Parking Plan for Downtown Harrisonburg completed in December 2009 and in the parking study authored by a Master of Public Administration student team from JMU. When creating or modifying parking facilities, designated bicycle rack areas also need to be provided.

As parking demand increases, it is recommended that:

- on-street parking be expanded where space exists or can be created through street redesign while enhancing the streetscape;
- City staff work with the development community to identify parking resources to accommodate both new development and redevelopment efforts;
- City staff explore and promote public-private partnership options (this could lessen the demand for on-street parking on particular streets allowing more flexibility for streetscape improvements);
- the Elizabeth Street parking deck be evaluated to determine if improvements to appearance and lighting, including the beautification of the walking route between East Elizabeth Street and Court Square, would increase usage during evenings and weekends;
- public surface lots include streetscape design features when improved or programmed for maintenance, and should include lighting, internal sidewalk/pedestrian access design, signage, and landscaping in accord with the Zoning Ordinance;
- existing parking lots be reconfigured where it allows for additional spaces; and that
- the City publicize and encourage greater utilization of parking lots on the fringe of the Downtown Core, including the Municipal Parking Lot. This can be accomplished, in part, with walkway and wayfinding improvements. While the Municipal Parking Lot is located close to the center of downtown, there is a perception that it is a farther walk due to the nature of the existing surroundings.

The City should consider the integration of residential and retail uses within future parking facilities. To lessen the demand for vehicle parking, it is recommended that public and private property managers be encouraged to integrate transportation demand management principles into their facilities, including provisions for the use of public transit and for short and long term bicycle parking with amenities such as showers, lockers, and bicycle repair stations into their facilities.

TRUCK ROUTING

With downtown being situated at the junction of two of the City's major arterial routes, U.S. Route 11 and U.S. Route 33, there is an inherent clash between the desire for access by large vehicles and the ability of the narrow downtown streets to accommodate them. Besides U.S. Route 42, U.S. Route 11 is the only other major north to south route that serves the City and is the primary detour route for Interstate 81. Also a primary Interstate 81 detour route, Route 33 is the only east to west route that stretches entirely through the City. Incidents routinely redirect interstate traffic through the City on these routes, causing heavy congestion and pushing a high volume of trucks into the narrow, pedestrian-heavy streets of downtown.

Of particular concern is the east to west routing of trucks during such incidents, as well as during routine deliveries and through trips across the City. For semi-trailers and other large trucks, traveling U.S. Route 33 around Court Square is a geometrically challenging path with tight turning radii and traffic signs often struck in the past by trucks.

To avoid repair costs for damaged street infrastructure, to improve traffic flow, and to foster a pedestrian-scale atmosphere in the downtown streetscape, it is recommended that trucks be encouraged to use routes that bypass downtown. A review by city staff has identified a favorable route that would relegate trucks to those streets with the best geometric accommodations and least impact on traffic patterns and residences. This route is depicted in the Recommended Truck Routing map.

Though this truck routing may be considered more favorable than other alternatives, it is not without its limitations that must be overcome. It is thereby recommended that the following four intersection improvements be considered to support safe truck turning movements and curtail damage to street infrastructure:

- **Martin Luther King, Jr. Way and South High Street** – enhance the westbound right turn radius on Martin Luther King, Jr. Way to northbound South High Street
- **North High Street and West Gay Street** – enhance the northbound right turn radius on North High Street to eastbound West Gay Street
- **East Market Street and Martin Luther King, Jr. Way** – enhance the northbound right turn radius on Martin Luther King, Jr. Way to eastbound East Market Street
- **North Mason Street and East Gay Street** – enhance the eastbound right turn on East Gay Street to southbound North Mason Street

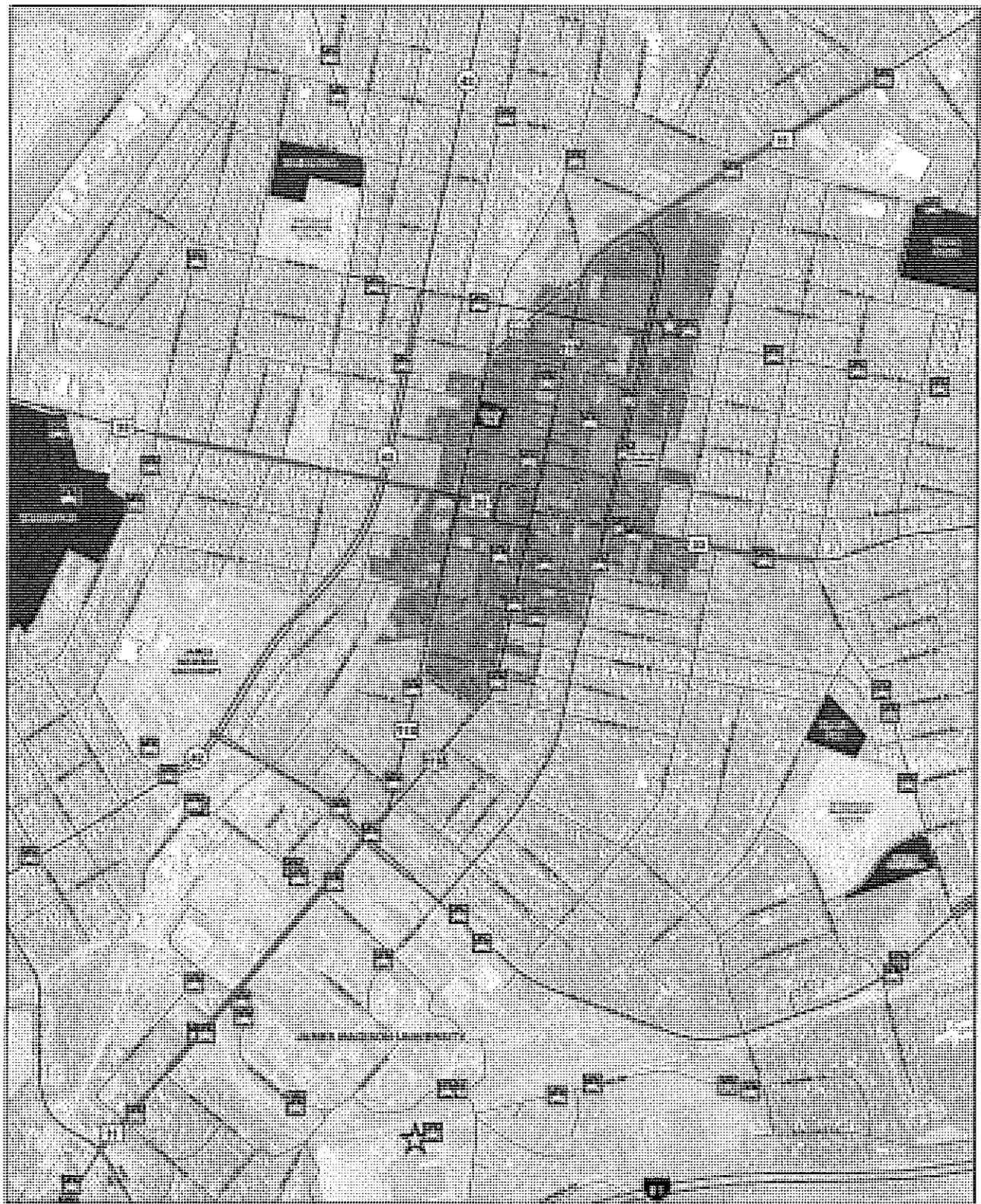
PUBLIC TRANSPORTATION

A reliable public transportation system is an important tool in the effort to reduce dependence upon personal vehicles. The Harrisonburg Department of Public Transportation offers bus services throughout the City, with the majority of its routes and ridership being focused in and around JMU. Four local transit routes service the downtown area, with on-demand paratransit also available. Historically, bus ridership downtown has been largely comprised of citizens living northeast of the Downtown Core in what is considered the Northeast Neighborhood. This area includes those living in the City's public housing managed by the Harrisonburg Redevelopment and Housing Authority (HRHA), many of whom rely upon buses as their primary means of transportation. The recent move of the downtown bus transfer station from East Bruce Street at the rear of the Hardesty-Higgins House into a large shopping center parking lot near the intersection of North Mason Street and East Gay Street has assisted in serving these users.

While it is beyond the scope of this plan to make judgments regarding the routing and ridership of the public transportation system, considerations as to how new downtown bus facilities can be made safer and be appropriately accommodated within the public right-of-way are highly relevant plan elements. The 2011 Transit Development Plan (TDP) is a six-year plan for transit services, resources, and funding opportunities that covers in depth the topics of bus ridership and routing.

It is necessary that public transportation accommodations be incorporated as a component of public and private development projects and that heavily used existing facilities be upgraded to ensure facilities properly serve the community and visitors alike all while coordinating efforts to improve the aesthetics of these public spaces and the adjacent streetscape. Bus stops that have been identified by the Department of Public Transportation as needing shelters installed in the near term include:

- Lineweaver Apartments on North Main Street near West Rock Street and
- Harrisonburg-Rockingham County Department of Social Services on North Mason Street near East Wolfe Street.



	Bus Stop
	Transit Center
	Train Station
	Bike Share
	Other Transit Facility

Transit Facilities
Downtown Streetscape Plan



CROSS SECTIONS

Maintaining, operating and planning to reconfigure streets in any downtown area presents many challenges. The core of Harrisonburg dates back to the late 1700's and is comprised of narrow streets and alleys, many of which were converted to accommodate one-way traffic in the last half of the 20th century. Providing a dependable, sustainable transportation network that is safe and accessible for all users is key to the economic vitality of downtown. Where possible, the City has worked to convert excessive pavement widths to accommodate new or wider sidewalks, but this effort often receives negative feedback from motorists and delivery drivers, and narrowing pavement is sometimes impossible on the narrowest of streets.

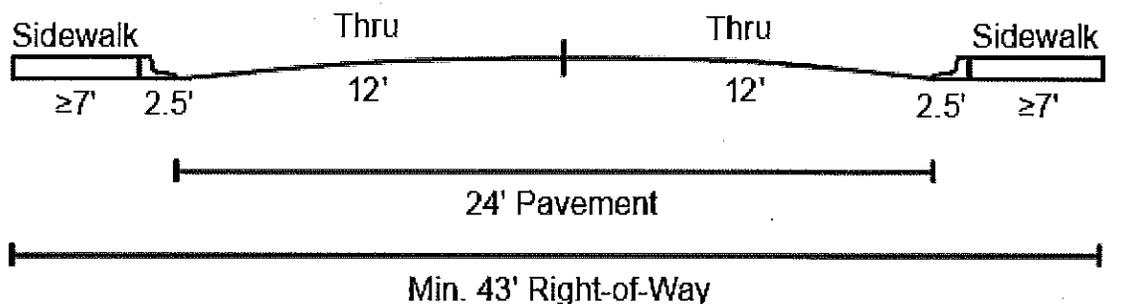
City staff has developed ideal cross-sections for various street types in the Downtown Core and Transition Area. While these are not achievable in all cases, they should be aspirational and serve as a guide when considering routine maintenance or new construction projects where lane configurations may be altered, such as street repaving.

While these cross-sections take into consideration multi-modal transportation needs, they do not show the sidewalk width needed to accommodate streetscape elements such as benches, trash cans, bike racks, street lighting, tree plantings, sandwich boards, etc. The more streetscape elements needed and desired, the wider the sidewalks will need to be to provide safe and comfortable passage for pedestrians.

These ideal street cross-sections are to be applied based upon the existing street classification (arterial, collector, or local) and the street's directionality. These classifications are depicted in the Transportation Overview map.

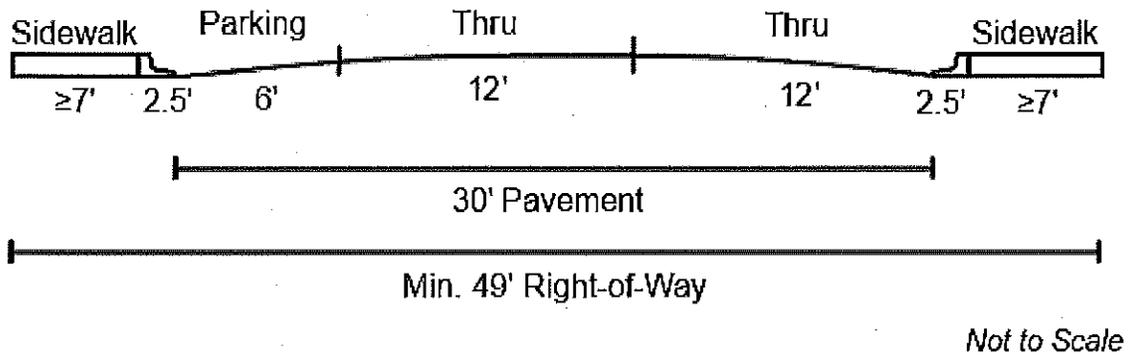
Typical cross sections for arterial streets in the downtown core:

Typical Section: Arterial Street - 1-Way, 2-Lane

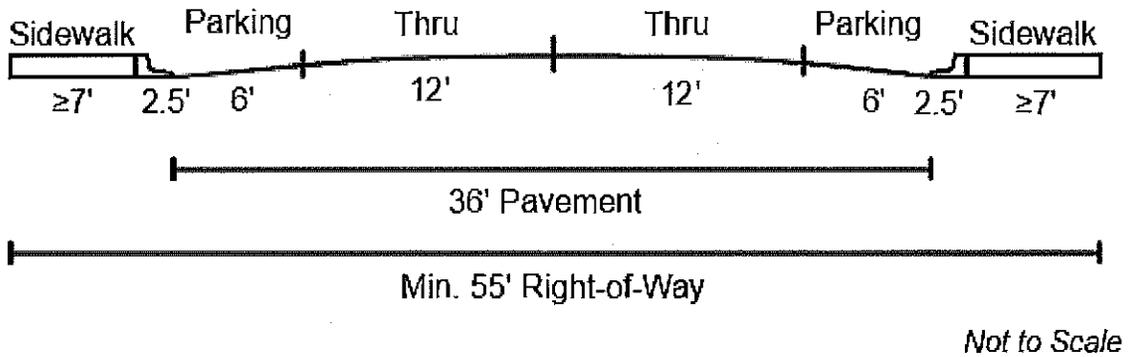


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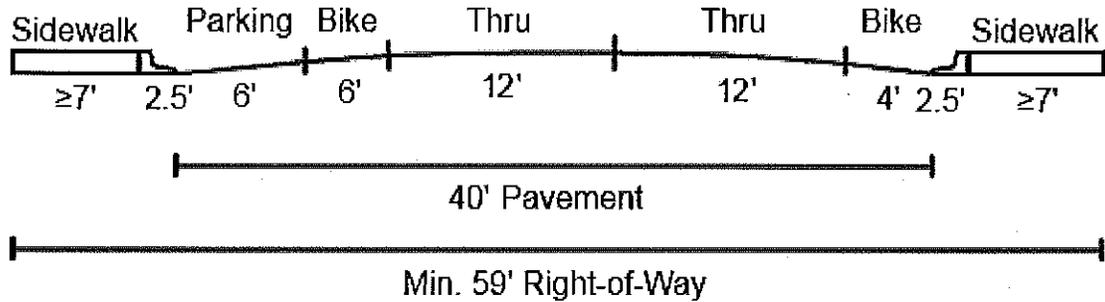
**Typical Section:
Arterial Street - 1-Way, 2-Lane with Parking**



**Typical Section:
Arterial Street - 1-Way, 2-Lane with Parking (Both Sides)**



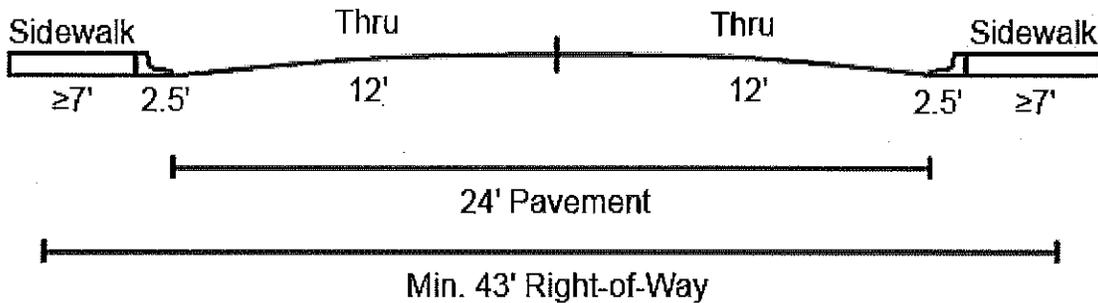
**Typical Section:
Arterial Street - 1-Way, 2-Lane, Parking, and Bike Lanes**



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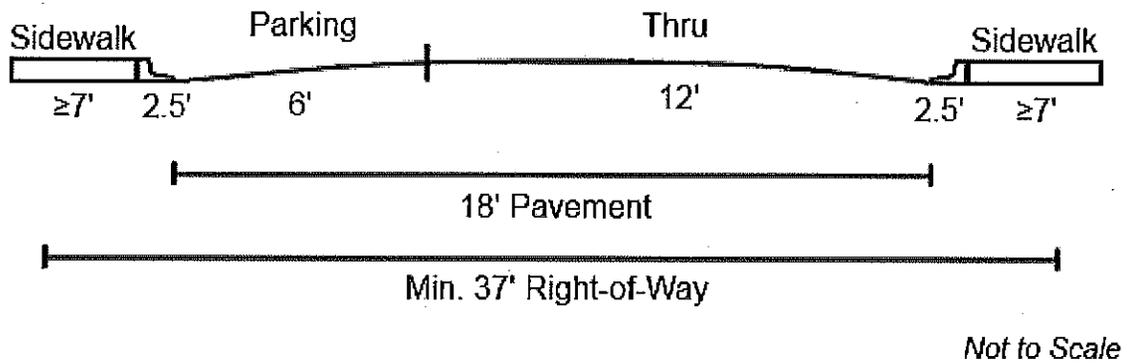
Typical cross sections for sub-arterial (local and collector) streets in the downtown core:

**Typical Section:
Local Street - 2-Way, 2-Lane**

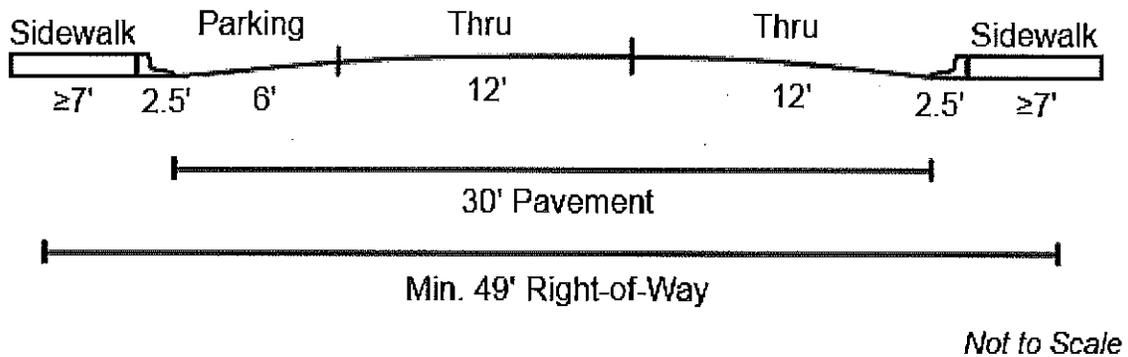


Not to Scale

**Typical Section:
Local Street - 1-Way, 1-Lane with Parking**



**Typical Section:
Local Street - 2-Way, 2-Lane with Parking**



DOWNTOWN GATEWAYS

The four corridors identified by this plan as gateways to the downtown area include North and South Main Street and East and West Market Street. These corridors provide direct routing into the Downtown Core and serve as the first impression of downtown. Vehicle capacity on these streets is generally sufficient under present conditions, but there are targeted improvements that could improve safety and efficiency, assist in supporting future traffic demand, and meet multi-modal user needs. It is recommended that attractive landscaping and public art projects be considered at each gateway to complement the World War I memorial on South Main Street.

NORTH MAIN STREET (U.S. ROUTE 11)

The North Main Street gateway is a 2-lane, 25 mph facility with sidewalks on both sides that runs from Ashby Avenue to Kratzer Avenue, where North Main Street splits into two one-way streets. This corridor carries the least amount of traffic when compared to the other downtown gateways, about 9,000 vehicles per day, and is primarily characterized by industrial complexes and other businesses, which contribute to high volumes of heavy vehicles. With only a single through lane in each direction, this section of Main Street acts as a bottleneck to traffic flow when vehicles are detoured from Interstate 81.

To improve traffic flow and enhance multi-modal capabilities, the City's Comprehensive Plan calls for the expansion of this street to a 3-lane facility and bike lanes. Since there are no major active or planned developments on this corridor, it is difficult to judge future transportation needs.

SOUTH MAIN STREET (U.S. ROUTE 11)

The South Main Street gateway is a 5-lane, 25 mph street with sidewalks and bike lanes on both sides that runs from Port Republic Road to just north of Martin Luther King, Jr. Way where it splits into two one-way streets. This corridor carries the most traffic to and from downtown relative to the other three gateways, as it is the primary north-south route in Harrisonburg and bisects the JMU campus. South Main Street is characterized by daily traffic of over 21,000 vehicles with high volumes of pedestrians and bicyclists, much of which is driven by JMU. Safe and efficient travel on the corridor is limited by a few factors, including the growing deficiencies in capacity on side streets such as Martin Luther King, Jr. Way and Grace Street, the high occurrence of mid-block pedestrian and bicycle crossings, and the oversaturation of traffic when class changes occur.

Side street capacity at the South Main Street & Grace Street and South Main Street & Martin Luther King, Jr. Way signalized intersections has become increasingly strained as JMU expands use of Memorial Hall, the former Harrisonburg High School on South High Street, and its recently acquired buildings and parking decks on the former Rockingham Memorial Hospital grounds off of Martin Luther King, Jr. Way and South Mason Street. JMU's Master Plan identifies East Grace Street as being a boulevard style street conducive to bicycling and walking, with the section east of South Main Street being open only to select motor vehicle traffic, such as the way Bluestone Drive currently operates. Such a change has the potential to divert and consolidate vehicles to other routes, such as South Mason Street and Martin Luther King, Jr. Way, furthering delays.

To mitigate these deficiencies, the City's Comprehensive Plan calls for the addition of a travel lane and raised, landscaped median on Martin Luther King, Jr. Way from South Main Street to approximately 300 feet east of Ott Street with bike lanes. This improvement would also include widening South Main Street at Martin Luther King, Jr. Way to add a northbound right turn lane.

Also recommended in the City's Comprehensive Plan is the construction of a landscaped median on South Main Street from Bluestone Drive to Port Republic Road, which could also involve replacement or relocation of water infrastructure where necessary, the installation of enhanced crosswalks, the upgrade of the aging traffic signals to new equipment with decorative styling, and the replacement of street lighting to the decorative style. In response to increasing safety concerns with mid-block pedestrian crossings, a median was installed in 2013 between Bluestone Drive/Warsaw Avenue and Grace Street. Extending this median to Port Republic Road would further improve transportation safety and greatly enhance the visual character of the gateway.

WEST MARKET STREET (U.S. ROUTE 33)

The West Market Street gateway is a 4-lane, 35 mph street with sidewalks on both sides that runs from Dogwood Drive to the railroad tracks just west of the intersection of Liberty Street & West Market Street. This corridor services about 11,000 vehicles per day and is characterized by fronting residential homes and neighborhood streets to the west of Route 42. To the east of Route 42, West Market Street narrows to two 25 mph lanes as it enters downtown. No improvements are identified for this corridor on the City's Comprehensive Plan and this plan has no recommendations for the gateway beyond the implementation of streetscape elements.

EAST MARKET STREET (U.S. ROUTE 33)

The East Market Street gateway is a 4-lane, 35 mph street with sidewalks on both sides of the street that runs from its intersection with Reservoir Street and Sterling Street to its intersection with Mason Street. This corridor services a daily traffic volume of around 14,000 vehicles. This corridor is characterized by its long frontage of Woodbine Cemetery, the large Urban Exchange mixed use development, small businesses, and residential streets. When approaching downtown from the east, the intersection of Reservoir Street and East Market Street offers a sweeping view of the Central Business District. With its many street and business entrances and lack of dedicated turning lanes, frequent left turns interrupt traffic flow and evoke hasty lane changes to bypass turning vehicles. As a result of these left turn movements and conflicts with thru vehicles, the effective capacity of this four-lane street may, at busier times of day, be reduced to that of a two-lane street. Safety is also a concern for bicyclists on this street since speeds are high and lanes are a narrower 11-ft width, as compared to the standard 12-ft width.

To improve motorist and bicyclist safety, calm traffic, and create an improved aesthetic, the City should consider reducing this corridor to a 2-lane street with a raised, landscaped median, left turn lanes at public streets, and bike lanes. This configuration would help smooth vehicle flow by providing dedicated lanes to control and isolate left turn movements. By removing the existing conflicts between thru and left turning vehicles, the overall efficiency of the street can be improved.

The City should also consider converting the signalized intersection of East Market Street & Mason Street to a roundabout to provide improved safety and the ability to make U-turns to access business entrances to which left turns are restricted by the median. By converting from a signalized intersection to a roundabout, a location can experience a 78 percent reduction in severe crashes and a 48 percent reduction in overall crashes according to the Federal Highway Administration. Preliminary engineering review for the roundabout and the lane reconfiguration of East Market Street between Reservoir Street and Mason Street concluded that a single lane facility could accommodate traffic volumes, providing an equivalent or better service level than exists today.



Conceptual drawing of East Market Street improvements between Mason Street (on left) and Reservoir Street/Sterling Street (on right)



East Market Street before improvements (looking west from Myrtle Street)



East Market Street after improvements (looking west from Myrtle Street)



East Market Street after improvements (viewed in, looking west from Myrtle Street)



E. Market Street/Mason Street Roundabout Concept



STREETSCAPE ELEMENTS

The use of streetscape design principles for public spaces is a means for providing visitors and residents with an attractive, cohesive environment that is conducive to the encouragement of non-motorized travel and helps define an identity for downtown. These principles, which treat streets as places not exclusively meant for mobility, but also for social gatherings and various activities, have a significant impact on how users perceive a space. The use of 'streetscaping' can help attract people to downtown, creating a sense of community and stimulating economic activity. Such improvements can be seen today in Harrisonburg's downtown on the east side of South Main Street between Bruce Street and Elizabeth Street, where Phase I of the Streetscape Project has already been completed.

To clearly communicate a vision for the downtown streetscape, design elements have been identified for each street in the study area for inclusion in both public and private projects, including maintenance efforts. It is recommended that the City consider the development of a landscaping plan for downtown.

The following Streetscape elements are defined in the Appendix for each section of the study area:

Paving and Curbs

- Sidewalks
- Crosswalks
- Bicycle facilities
- Street paving
- Curbs

Street Furnishings

- Lighting
- Traffic signals
- Bus shelters/stops
- Bicycle racks
- Signage
- Landscaping
- Trash & recycling receptacles
- Benches

III. SERVICES & UTILITIES

Utilities and public services play a vital role in sustaining residences and businesses. By fostering customer focused services and reliable utilities, businesses are encouraged to locate and remain within the City. While the City offers a wide array of services to its citizens, solid waste management is the only service within the scope of this plan due to its uniquely challenging operations in the downtown area. Both public and private utilities are addressed to coordinate upgrades and expansions with street projects and to identify opportunities for infrastructure changes that would improve the character of the downtown environment.

To further public facility maintenance and enhancement efforts, the City may need to consider the use of alternative financing, such as a downtown service tax district as a way to fund existing and planned services and public facilities that are provided specifically for the downtown area, such as more frequent refuse collection, maintenance of landscaping, and maintenance of public parking facilities. These funds could also be used to upgrade or expand the aging water and sewer infrastructure downtown, an undertaking that poses major budgetary and logistical challenges. An example of a similar tax district can be found in Roanoke, VA, where the "Downtown Service Tax District" provides the following services:

- Economic and business development
- Promotional activities intended to foster business retention
- Business recruitment and developer recruitment
- Planning for the development or revitalization of downtown
- Transportation and public facility and public space needs

SOLID WASTE MANAGEMENT

The space limitations and lack of indoor or outdoor refuse storage areas has created issues for city services and businesses downtown. Though refuse is collected daily between Monday and Friday by the City, many restaurant establishments require additional trash collection on the weekends due to the high volume of customers served during that time. Since no ordinances are in place to require customers to account for refuse space inside their premises, trash is often left cluttered on sidewalks while it awaits Monday pickup, creating safety, health, and curb appeal issues. Customers within the Central Business District currently receive daily collection at the same rate structure as all other customers in the City who are provided once per week collection.

To address this growing problem, the City met with local downtown business leaders in 2012 to discuss multiple options including a weekend collection program. After months of discussing options, it was deemed that a weekend collection program was not a financially viable option for the City due to the limited number of downtown businesses interested in paying additional fees for the extra collection. The City will, however, monitor the downtown area on weekends to ensure violators are cited and fined according to City ordinances.

In addition to the weekend monitoring program, it is recommended that alternative collection programs be considered along with an ordinance change to require all downtown developments and redevelopments to include space to accommodate refuse for up to 3 days, thus providing room for waste buildup during the weekend and holidays until pickup can occur.

The following projects are under consideration to enhance solid waste services downtown and to promote the four R's of the waste hierarchy (Reduce, Reuse, Recycle, and Recover):

- Pay-As-You-Throw (PAYT) Program - Utilize City provided carts and cart dumpers on refuse trucks equipped with weighing software, which would streamline the waste receptacles downtown and rid the area of loose trash bags.

- Promote the use of curbside recycling for businesses that are currently choosing not to recycle.

UTILITIES

There are a number of public and private entities possessing existing utility infrastructure or having an interest in future installations in the downtown area. These stakeholders include:

- Harrisonburg Electric Commission (electricity)
- City Department of Public Utilities (water, sewer)
- City Department of Public Works (stormwater, traffic)
- Columbia Gas of Virginia (natural gas)
- Comcast (cable line services)
- Verizon (phone line and cellular services)
- Lumos Networks (previously nTelos – phone line and cellular services)
- Shentel (phone line services)

Private utility companies were contacted to request information about existing and planned infrastructure, but they were unable to provide any information due to security and competition concerns. For this reason, it is imperative that plans for streetscape improvements continue to be shared at the quarterly utility coordination meetings between City staff and private company representatives. Utility retrofits should be sensitive to both existing and planned streetscape improvements. It is recommended that a special review process for downtown be created through the City's Public Access Permit process for use when utility companies prepare for upgrades or replacements. This will help ensure the appropriate City departments are notified of the upcoming work.

Upgrades to public utilities, including electricity (semi-private), water, sewer, stormwater, and traffic should also be coordinated with streetscape improvements. In keeping with past efforts to help improve the visual character of downtown, placing utilities underground where feasible and appropriate is encouraged. To help facilitate this effort, consideration should be given to including conduit installation as a component of public street and sidewalk projects, which could then be leased to utility companies.

GREEN INFRASTRUCTURE OPPORTUNITIES

Blacks Run flows through the heart of downtown Harrisonburg. It is a stream that originates in the northern parts of the City and flows south where it enters downtown at West Washington Street and exits downtown where Chesapeake Avenue crosses under Martin Luther King, Jr. Way. Eventually, Blacks Run flows into Cooks Creek, North River, Shenandoah River, Potomac River, and finally the Chesapeake Bay. During heavy rainfall, large amounts of water flows across impervious surfaces in downtown and into storm sewer systems, and then into Blacks Run, which causes localized flooding in some areas and erosion of stream banks that contributes to heavy sedimentation of local waterways.

Green infrastructure uses vegetation and soil to manage rainwater where it falls and can contribute to healthier waters. Green infrastructure encompasses a variety of techniques that replicate and restore the natural hydrologic cycle and reduces the volume of stormwater entering the storm sewer system and into Blacks Run. Green infrastructure generally includes stormwater management methods that:

- Infiltrate (porous pavements, sidewalks, and gutters; linear infiltration systems)

- Evaporate, transpire and reduce energy consumption (vegetated roofs, trees, planter boxes)
- Infiltrate and transpire (rain gardens and bioretention)
- Capture and reuse rainfall (rain barrels, cisterns, irrigation supply systems, and gray water systems)

In contrast to traditional gray infrastructure, which refers to traditional practices for stormwater management and wastewater treatment such as pipes and sewers, a green infrastructure approach preserves and restores natural landscape features when possible, and uses technologies that infiltrate, evapotranspire, capture, and reuses stormwater. Green infrastructure approaches often have higher returns of investment and offer multiple benefits, including:

- *Environmental* – recharges ground water, provides natural storm water management, reduced energy usage through mitigation of the heat island effect, and improved water quality.
- *Social* – beautifies and increases recreational opportunities, improves health through clean air and water, and improves psychological well-being.
- *Economic* – reduces future costs of stormwater management, reduces potential for localized flooding events and damage to property and public infrastructure, reduces cost of treating water for drinking downstream, and increases property values.

Opportunities to install green infrastructure with redevelopment and improvement projects in downtown might include:



Planter boxes: Urban rain gardens with vertical walls and open or closed bottoms that collect and absorb runoff from sidewalks, parking lots, and streets.



Bioswales: Vegetated, mulched, or xeriscaped channels that provide treatment and retention as they move stormwater from one place to another. As linear features, vegetated swales are particularly suitable along streets and parking lots.



Permeable pavements: Paved surfaces that infiltrate, treat, and/or store rainwater where it falls. Permeable pavements may be constructed from pervious concrete, porous asphalt, permeable interlocking pavers, and several other materials.



Green streets and alleys: Include integrating green infrastructure elements into the street and/or alley design to store, infiltrate, and evapotranspire stormwater.



Green parking: Includes integrating green infrastructure elements into parking lot designs. Permeable pavements can be installed in sections of a lot and rain gardens and bioswales included in medians and along parking lot perimeters.



Urban trees: Reduce and slow stormwater by intercepting precipitation in their leaves and branches. Other benefits include urban heat island mitigation and a more walkable built environment.

Source: http://water.epa.gov/infrastructure/greeninfrastructure/gi_what.cfm

IV. LAND USE & COMMUNITY DEVELOPMENT

Well defined policies for land use and desired quality of development consistent with the vision for the surrounding environment are an essential part of sustaining a vibrant downtown community. City officials work to achieve this through the use of the Comprehensive Plan's Land Use Guide, the Subdivision Ordinance, the Zoning Ordinance, and the Design & Construction Standards Manual. The City's vision for land use is defined in the Comprehensive Plan. Both the existing and future uses of land in downtown and the immediately surrounding area play a role in street and streetscape design. The Comprehensive Plan recommends the majority of properties in the study area for mixed use development, which correlates with the B-1 Central Business District and MX-U Mixed Use Planned Community District classifications in the Zoning Ordinance. We have already begun to experience this with rezoning requests of traditionally industrial properties along the Chesapeake & Western Railway to B-1 - aligning with traditional development standards associated with downtown.

The City has assumed a strong economic development perspective with regard to downtown revitalization, which includes the creation of several incentive programs. More details for these incentives can be found in the Harrisonburg City Code. The currently available incentives are:

- Harrisonburg Downtown Technology Zone - created to encourage technology companies to locate in a limited area of downtown. Incentives include water and sewer connection fee exemptions for three years along with Business Professional & Occupation License fee relief.
- Economic Revitalization Zone - includes all parcels located within the B-1 zoning classification and Virginia's Main Street District. Tax incentives include partial exemption for 5-10 years from real estate taxation for new commercial and residential mixed-use construction exceeding \$1 million and containing a minimum of 40 percent retail on the ground floor.
- Central Business District Tax Incentive - offers partial exemption from real estate taxation for up to 5 years for qualifying buildings at least 25 years old that receive substantial rehabilitation.
- Arts and Cultural District - first district in Virginia to be created in order to promote a vibrant downtown arts related atmosphere. Tax incentives include exemption from admissions taxes and Business Professional & Occupation License fee for qualified businesses and organizations.
- Downtown Historic District - provides state and federal tax credits for approved renovation and restoration of qualifying historic buildings at least 50 years old.

There are many opportunities to nurture revitalization and development within the Downtown Core and Transition Area. One such opportunity is to maintain a relationship between the City and the community by continuing to work with downtown alliances and organizations, such as Harrisonburg Downtown Renaissance, the Downtown Dining Alliance, and neighborhood groups. This will help further common goals that protect property values, further economic development interests, and minimize impacts to public resources.

With key downtown properties available for mixed-use development in the downtown core, it is also important that the City partner with private property owners and community stakeholders to provide opportunities for development or redevelopment. It is important to note that, while it is not the goal of this plan to define standards for historic preservation, development, or redevelopment, the business community is an integral component to achieving this plan's goals.

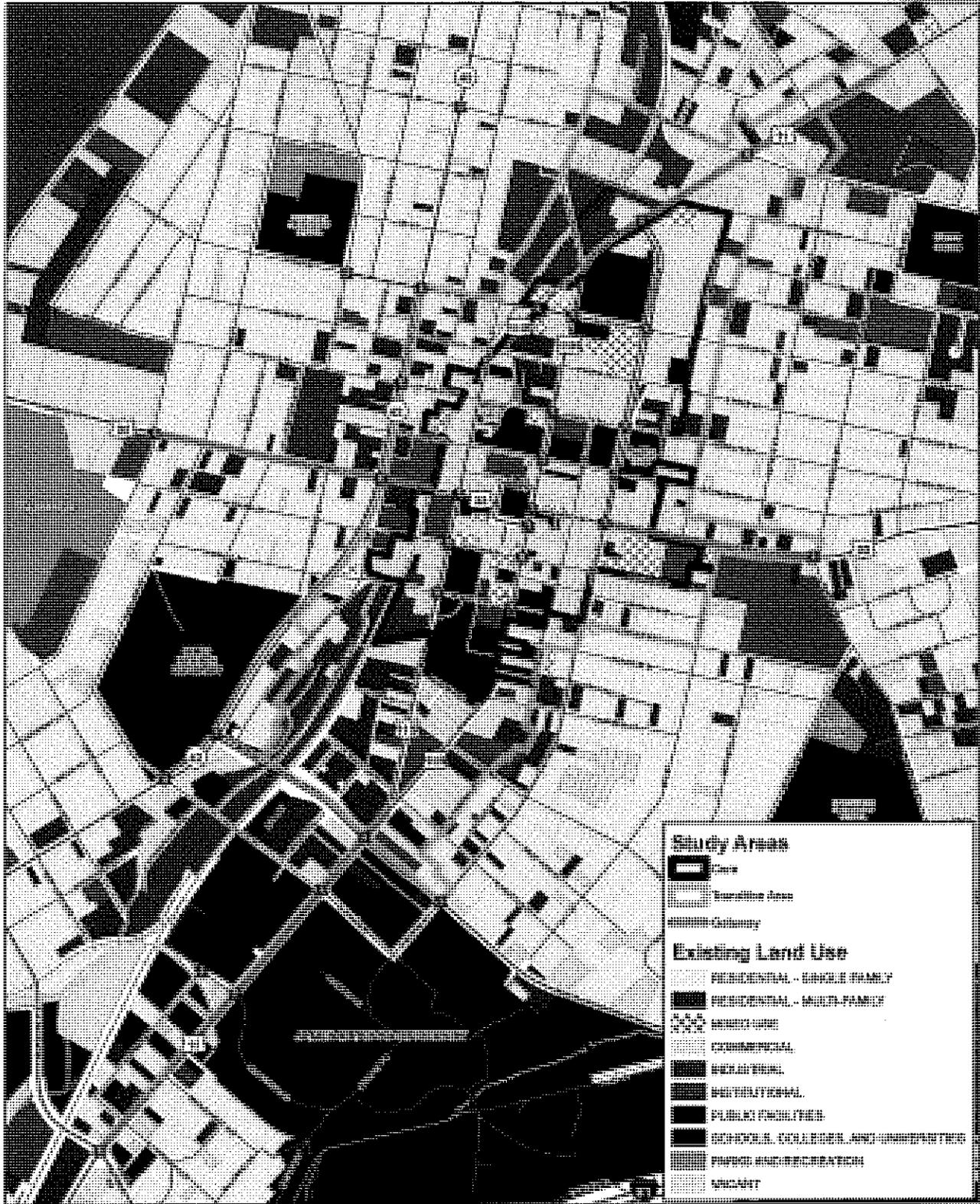
One revitalization opportunity may be found with the Chesapeake Avenue corridor. As JMU expands into the Grace Street corridor as indicated on their Master Plan, Chesapeake Avenue could be redeveloped as a connection between JMU's main campus and downtown.

To further support goals for expanding public recreation and nurturing a mixed-use lifestyle downtown, the City should consider converting Federal Street to a one-way street with a wide shared use path that includes attractive landscaping and public art. Due to its low traffic volumes and narrow pavement, Federal Street lends itself well to providing a safe, family friendly, recreational corridor that runs much of the length of the downtown core. By connecting the Federal Street path to existing north and south greenway trail concepts, a pedestrian and bicycle corridor could link many residential areas in Harrisonburg to the downtown core. This concept was discussed as a transportation component, but it is important that the possible economic and community benefits of such a facility be taken into consideration.

Although it is small, and despite the fact that it has been "tunneled" under many streets, buildings, and parking lots, Blacks Run has the opportunity to provide scenic beauty in a variety of contexts.

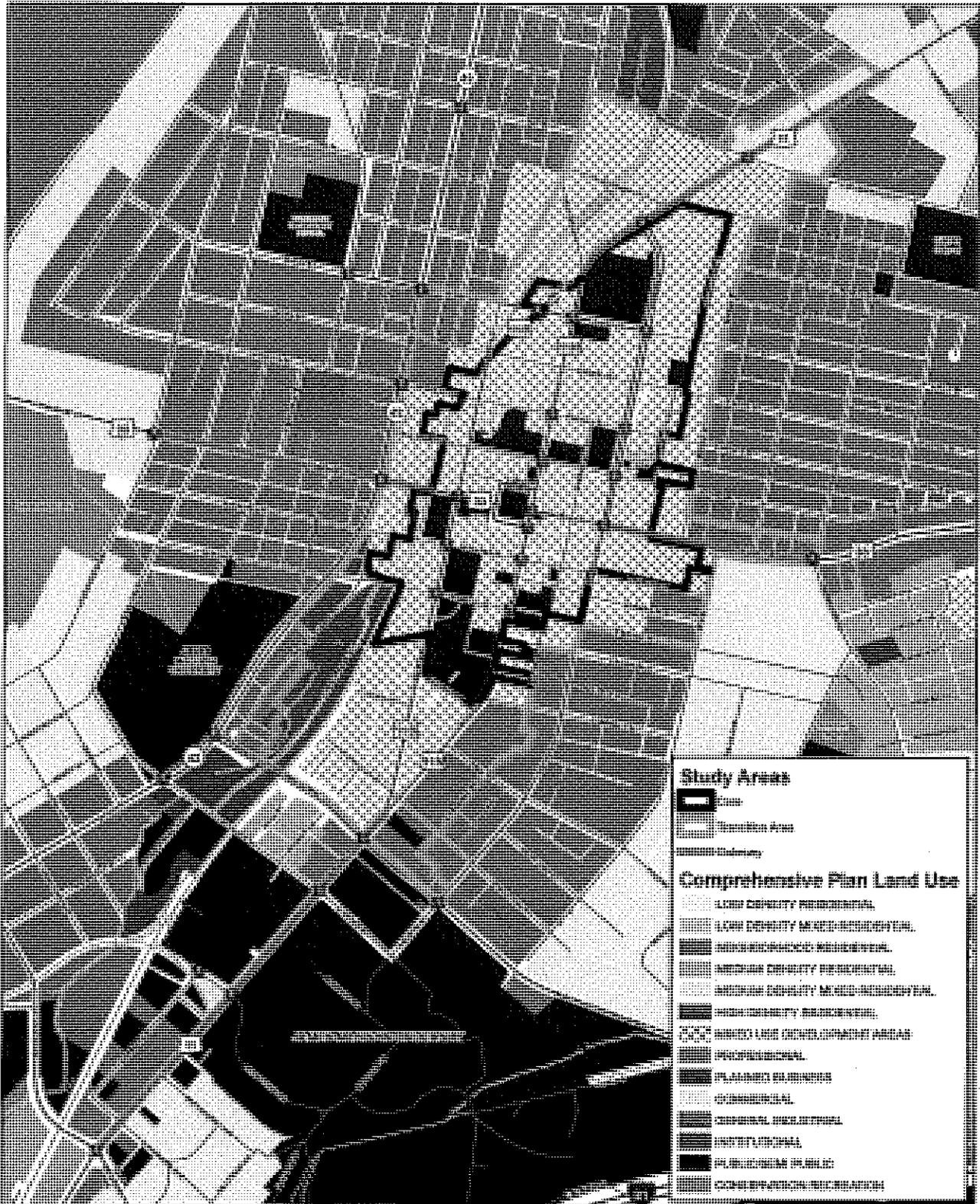
- Between Gay and Rock Streets, Blacks Run winds through a narrow, mainly privately owned, green area below grade level – an area that could be enhanced through landscaping.
- At Liberty Park, a stream restoration project was completed in 2006 and Blacks Run provides a beautiful vegetated and shaded area for park visitors to relax in and enjoy a connection to nature along the stream. The park and surrounding area are publicly owned.
- Between Court Square and West Water Street, there exists a pedestrian walkway in a public easement that overlooks Blacks Run and adjacent restaurant owners have taken advantage of outside dining opportunities and have vegetated the opposite stream bank with a mixture of native plants.
- Behind Shenandoah Bicycle Company there is a small privately owned pocket park that provides an intimately scaled green space with trees, ornamental plantings, a path, and picnic table. This area could be enhanced by cleanup and removal of overgrown invasive plant species.

Other opportunities may be available for nurturing the economic benefits that Blacks Run can bring to a downtown area. Projects such as Liberty Park and the pocket park behind Shenandoah Bicycle Company resulted from public-private partnerships between the City, community groups, and private property owners who own the banks and areas adjacent to Blacks Run.



Existing Land Use
Downtown Streetscape Plan

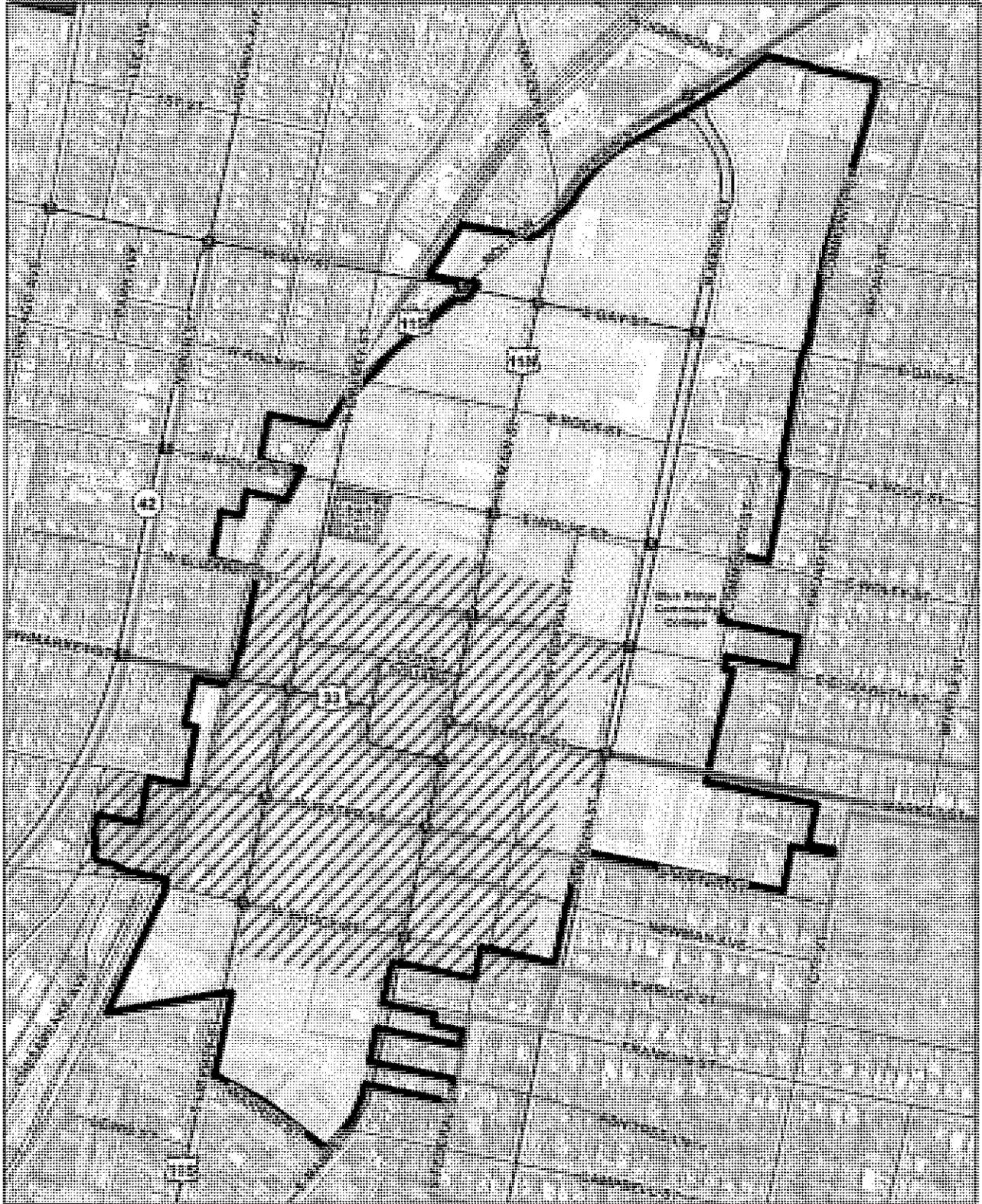




2011 Comprehensive Plan Land Use Guide

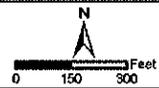
Downtown Streetscape Plan





Economic Development Zones
 Economic Revitalization Zone
 Downtown Technology Zone
Study Areas
 Core
 Transition Area
 Gateway

Economic Development Zones Downtown Streetscape Plan



V. IMPLEMENTATION STRATEGIES

Objective 1: Develop a comprehensive vision for the public right-of-way within the study area that defines public and private expectation for project elements during development and redevelopment of properties.

- a. Use cross sections for streets to provide specific recommendations for improvements.
- b. When properties are redeveloped or rezoned, consider sidewalk replacement and other utility replacement to be compatible with downtown design elements.

Objective 2: Provide a plan for safe and efficient pedestrian and bicycle accommodations that enhance the public streetscape and related public spaces.

- a. Use cross sections for specific improvement recommendations on each street.
- b. Design downtown streets using "complete streets" principles where possible.
- c. Encourage routing and complete necessary intersection improvements to relegate trucks to more appropriate streets outside the downtown core.

Objective 3: Expand parking opportunities in coordination with streetscape enhancements to support future downtown business, residential housing, and visitor needs.

- a. Expand on-street parking where space exists or can be created through street redesign.
- b. Encourage greater utilization of parking lots on the fringe of the downtown core, such as the Municipal Parking Lot, with walkway and wayfinding improvements.
- c. Expand bicycle parking facilities to encourage and accommodate bicycling downtown.
- d. Evaluate existing parking facilities to ensure they are easily accessible and attractive to downtown visitors to encourage their full usage.

Objective 4: Enhance public transportation facilities to accommodate citizens and visitors in coordination with streetscape improvements.

- a. Integrate public transit accommodations and facilities to serve residents of downtown, as well as providing transit accommodations that make downtown a destination for visitors and community residents.
- b. Install bus shelters at high volume stops.
- c. Encourage public transportation accommodations as a component of public and private development projects.

Objective 5: Develop a plan for public services in the downtown area that addresses the changing needs of businesses.

- a. Continue work to develop a solution for providing trash pickup on weekends.
- b. Evaluate the use of larger trash receptacles to prevent the storage of trash on sidewalks while awaiting pick-up.
- c. Promote the use of public recycling service for businesses that are currently choosing not to recycle.
- d. Evaluate the concept of requiring downtown development and redevelopment projects to accommodate trash storage for up to three days.
- e. Consider the use of a downtown service tax district to help fund additional services provided for the downtown area, such as more frequent refuse collection, maintenance of public parking facilities, and maintenance of landscaping.

Objective 6: Plan for necessary improvements to public and private utility infrastructure so that does it not impede or encumber streetscape improvements.

- a. Complement known and needed upgrades with utility upgrades and new service expansion. Retrofits should be sensitive to any existing or planned streetscape improvements.
- b. Create a special review process through the Public Access Permit process to be used when utility companies prepare for upgrades or replacements.
- c. Consider the elimination of overhead utility lines where feasible.
- d. Considering utilizing green infrastructure practices whenever feasible.

Objective 7: Partner with property owners and community stakeholders to provide opportunities for development or redevelopment of public and private downtown properties that could enhance the public streetscape.

- a. Work with James Madison University (JMU) to better connect the main campus with downtown. As JMU redevelops the Grace Street corridor, the Chesapeake Avenue corridor should be considered for redevelopment in partnership with private property owners as a connection between JMU and downtown.
- b. Partner with JMU to extend the South Main Street median, which would contribute to developing an attractive gateway linking the university campus to downtown.
- c. Partner with technology developers, an attractive industry sector desired to populate downtown as evidence by the City's Downtown Technology Zone and applicable incentives, to maintain their presence in the downtown area, and provide a "campus-like" atmosphere that integrates ideals and principles of the company.
- d. Continue partnerships with developers to revitalize vacant properties.
- e. Work with downtown organizations to further common goals that protect property values, further economic development interests and minimize impacts to public resources.

Objective 8: Consider expansion of recreational and open space opportunities downtown that coordinate with public streetscape improvements.

- a. Consider developing a plan for a recreational, shared use path using the Federal Street corridor to tie into greenway concepts to the north and south of downtown.
- b. Consider developing a plan for a recreational, shared use path between the Harrisonburg Municipal Center and West Bruce Street, and a pedestrian path between West Bruce Street and Liberty Park.

Objective 9: Enhance the visual character of the downtown streetscape.

- a. Integrate design elements of downtown features into maintenance projects as per streetscape standards.
- b. Consider the development of a public landscaping plan for downtown.

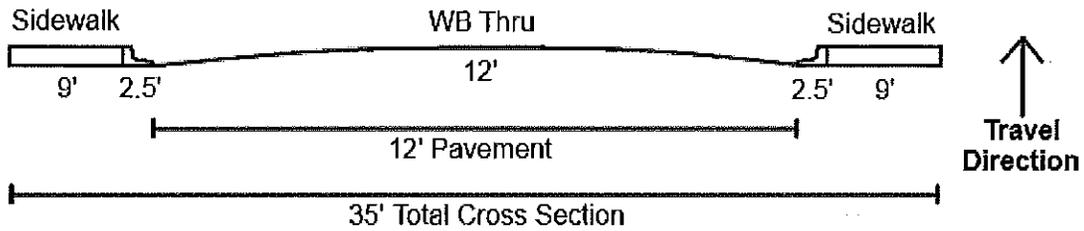
VI. APPENDICES

APPENDIX A: STREET SPECIFIC RECOMMENDATIONS

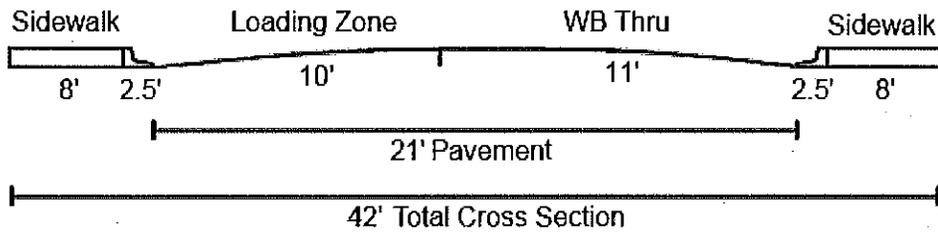
More detailed cross-sections have been created for streets with specific redesign goals addressed in this plan. For streets without specific recommendations, the typical cross sections should be consulted. Corridor specific recommendations are as follows:

- Water Street – A popular commercial corridor, Water Street serves as a link between the most heavily utilized public parking resource and the majority of attractions found in the downtown core. Being a one-way street with few driveways, vehicle capacity is well accommodated by a single lane. Opportunities for the widening of sidewalks to enhance pedestrian travel and the accommodation of delivery vehicles should be sought as priorities for this corridor.

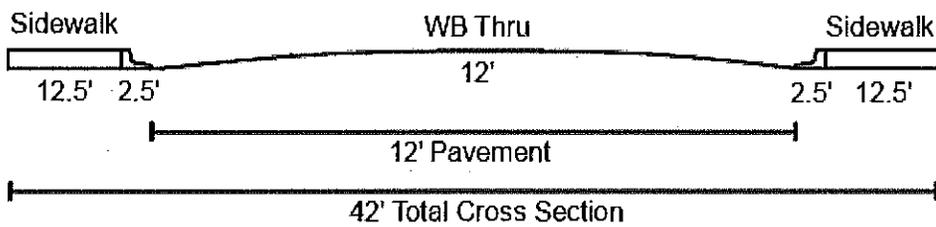
S Main St - Bank of America Alley



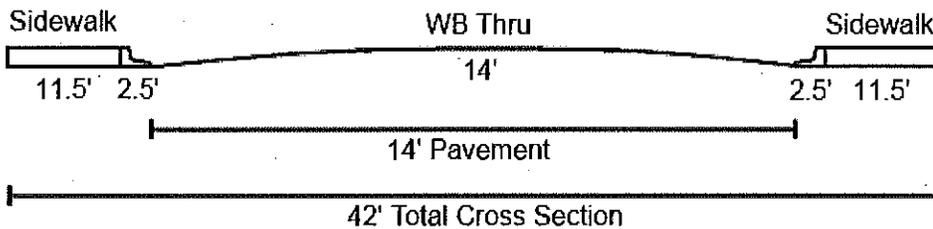
Loading Zone (In Vicinity of Free Clinic)



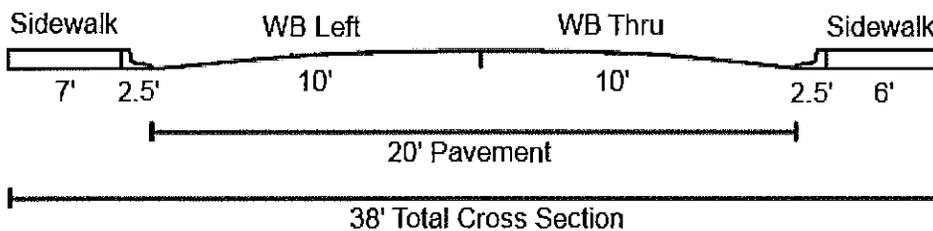
Bank of America Alley - East End of Parking Deck



East End - West End of Parking Deck



West End of Parking Deck - S Liberty St



Not to Scale

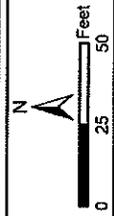
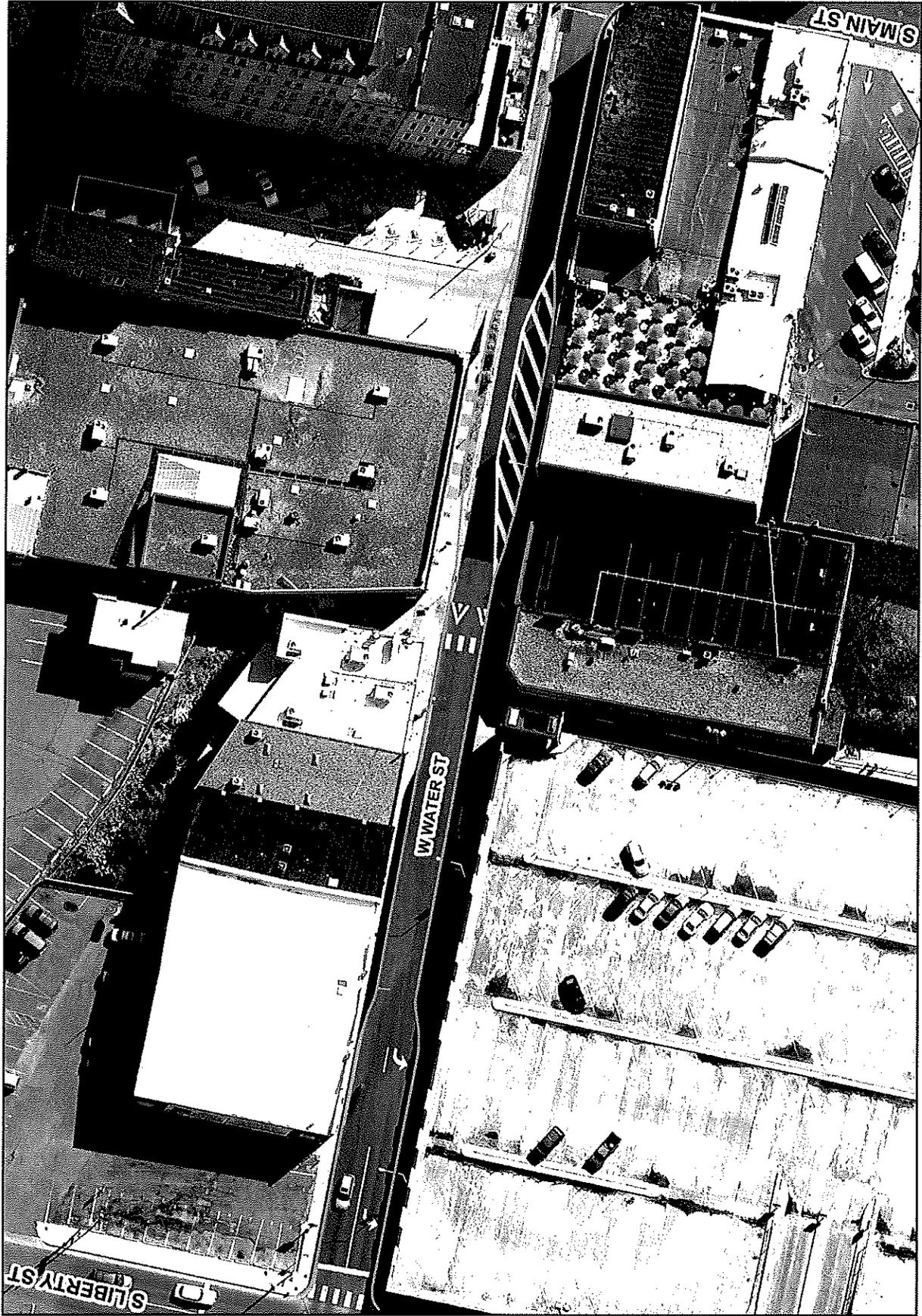
West Water Street cross sections (South Main Street to South Federal Street)



West Water Street before improvements (looking west from South Main Street)



West Water Street after improvements (looking west from South Main Street)



West Water Street Realignment

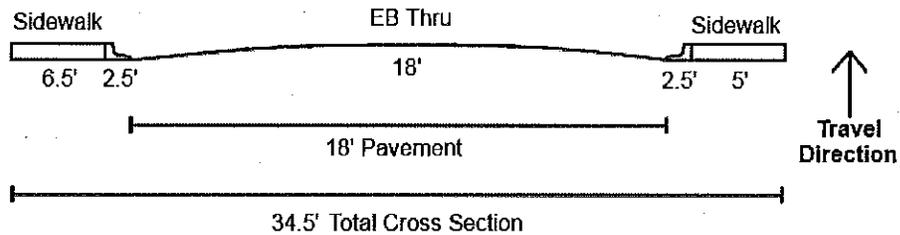
Downtown Streetscape Plan

-  New Curb Alignment
-  Sidewalk Expansion
-  Delivery Zone

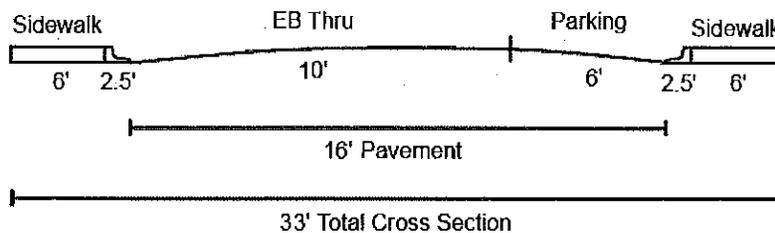
- **Bruce Street** - Serving primarily as an access street to the Water Street parking deck and the Municipal Parking Lot and a connection to South Mason Street from the west with few destinations, the Bruce Street corridor's role leans heavily towards vehicular access. While pedestrian accommodations should be enhanced in areas, the greatest opportunity for redesign would be the addition of on-street parking west of South Main Street.

Bruce Street Cross Sections (S Liberty St - S Mason St)

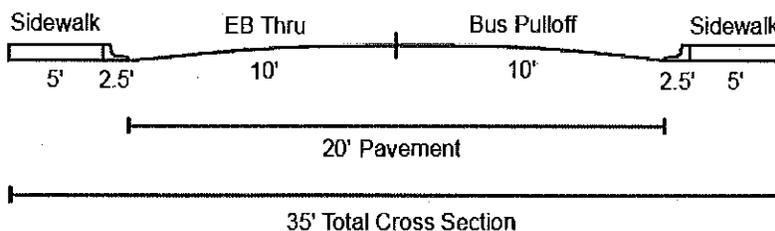
S Liberty St - West side of Blacks Run bridge



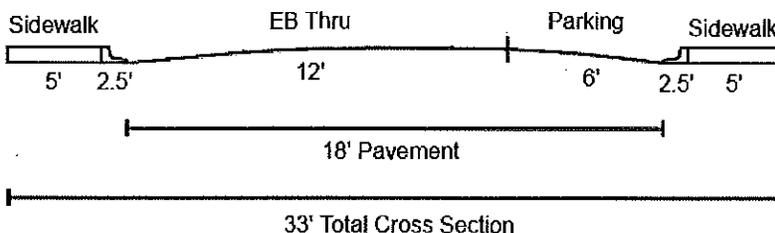
West side of Blacks Run bridge - S Main St



S Main St - East end of Hardesty-Higgins lot



East end of Hardesty-Higgins lot - S Mason St



Not to Scale

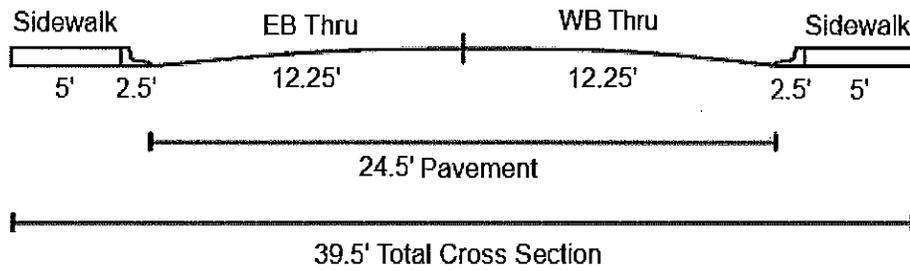
East Bruce Street cross sections

- Gay Street - A two-way east-west arterial, Gay Street provides a high capacity connection between South High Street (U.S. Route 42) and North Mason Street and is a primary connection to downtown by those living in the Northeast Neighborhood (framed by North Mason Street, North Main Street, East Washington Street, and East Market Street).

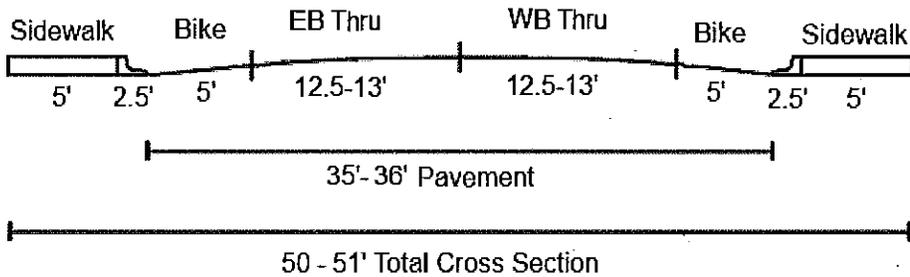
While more than a single queuing lane is needed at major intersections, the wide, 4-lane midblock sections of Gay Street are overbuilt for the volume of traffic they carry and offer an opportunity for adding bike lanes and shared lane markings ("sharrows"). This plan differs from the plan shown in the City's 2010 Bicycle & Pedestrian Plan (amended 2011). The Bicycle & Pedestrian Plan shows Wolfe Street as the east to west connector and proposed installing sharrows. This plan suggests the use of bike lanes and sharrows on Gay Street instead of Wolfe Street as the east-west connector. Gay Street will provide bicyclists with a route on slightly wider streets, which will help bicyclists avoid the "door zone" of cars parked on-street; and will provide bicyclists with a route with significantly fewer stop signs so they may conserve energy that would have been used to stop at every intersection on Gay Street. The Bicycle & Pedestrian Plan will be revised to reflect this change during its 2015 update.

The opportunity for constructing a westbound right turn lane at North Main Street should be explored to avoid the need for a sharp lane transition.

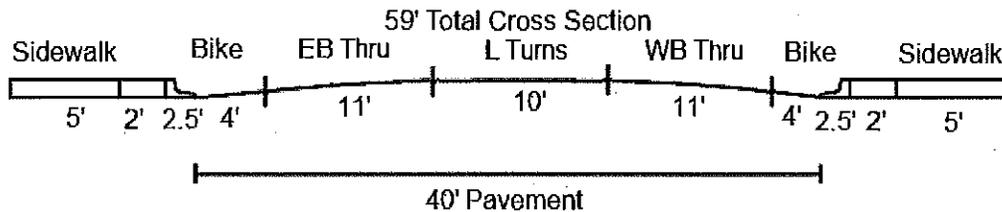
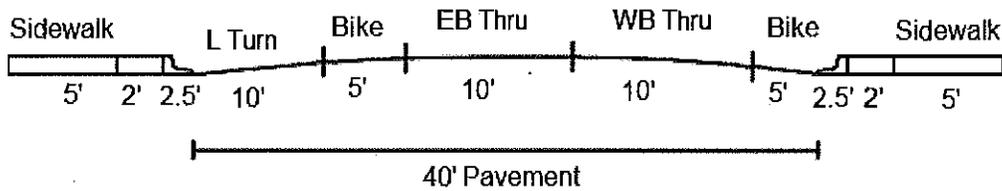
N High St - N Liberty St



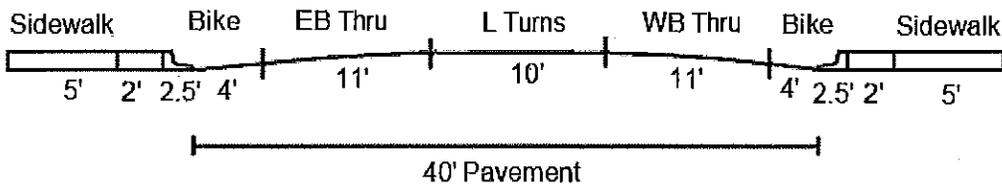
N Liberty St - N Main St



N Main St - N Mason St



N Mason St - Broad St



Not to Scale

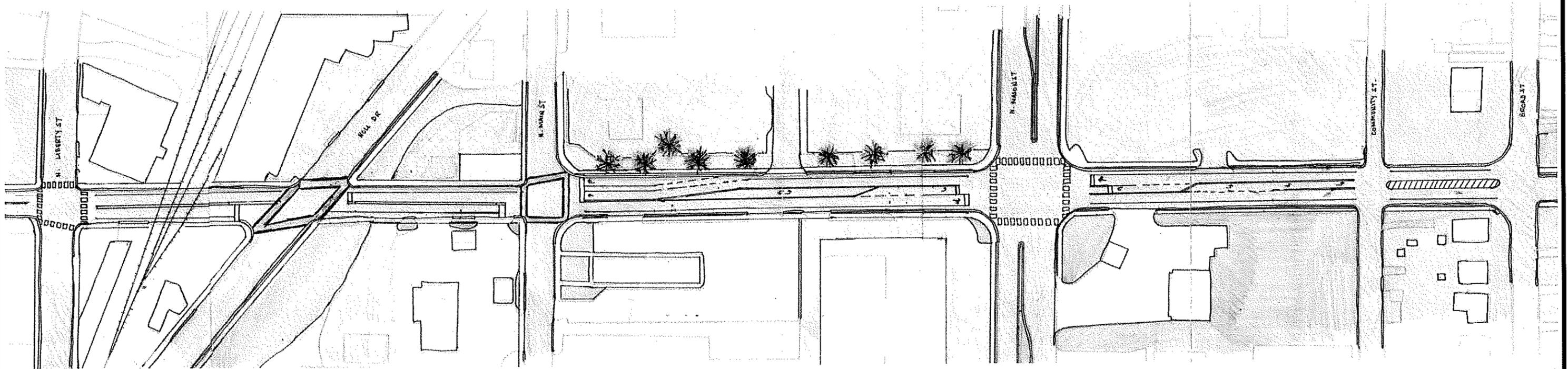
Gay Street cross sections



East Gay Street before improvements (looking west from North Mason Street)



East Gay Street after improvements (looking west from North Mason Street)



Conceptual drawing of Gay Street improvements between North Liberty Street (on left) and Broad Street (on right)

- South Main Street – South Main Street is the primary entrance to the downtown and provides two travel lanes, on-street parking in sections, and shared lane markings for bicyclists. South of Campbell Street, the street widens and provides unmarked, two-sided on-street parking. Due to this expanded width, this section experiences higher than desired travel speeds and longer pedestrian crossing distances. To meet the goal of fostering a pedestrian-scale atmosphere on this street, it is recommended that curb extensions be installed at the intersections of Grattan Street, Paul Street, Campbell Street, and Franklin Street.

Curb extensions have the following features:

- They narrow the street, contributing to the calming of vehicular traffic.
- They shorten the distance that a pedestrian must travel to cross a street. Pedestrians will feel safer.
- They increase the sight distance between the motorist and pedestrians crossing the street.
- They improve sight lines for vehicles on side streets.
- They create additional pedestrian space that can be used for streetscape elements.

Appropriate accommodations must be made for drainage facilities when installing these features. Planters may be an attractive addition within the extended sidewalk area provided they do not block sight lines for vehicles on the side streets. To assist with visually narrowing the street, on-street parking spaces should be marked on South Main Street between East Grattan Street and the existing parking stall markings to the north.



South Main Street before curb improvements (looking north at Campbell Street)

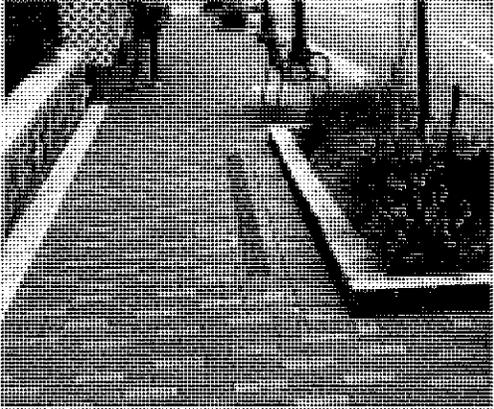


South Main Street after curb improvements (looking north at Campbell Street)

DESIGN STANDARDS & DESCRIPTIONS

Below are standards and details for the downtown streetscape. Additional details may also be available from previously completed downtown streetscape engineering plans. Previous engineering plans should be reviewed to ensure consistency between existing and planned facilities. Locations for certain features are defined in the Streetscape Standards map.

Additionally, given that this Plan is conceptual, further coordination with adjacent property owners and emergency responders will be done during further planning and design engineering.

Sidewalks	
Brick Sidewalk	 <p>Sidewalk Brick Pavers will use the Pine Hall Standard 2 1/4" X 4" X 8" square edge paving brick. Mix Pine Hall Pathway Full Range Bricks with Pine Hall Rosewood Full Range Bricks in a ratio of 3 pathway to 1 rosewood. Avoid setting more than 2 rosewood style bricks together.</p>
Decorative Concrete Sidewalk	 <p>Standard A-3 or A-4 with "Old Virginia Shale" color tint from Superior Concrete (540) 434-0346 or equivalent from other supplier. Other suppliers may name this shade differently and the tint may be different depending on the supplier.</p> <p>Contractor to coordinate border width for rectangular print with Public Works prior to beginning work.</p> <p>*Note: To accommodate Fire Department ladder trucks, where pavement widths are less than 20-feet, then adjacent sidewalks must be a minimum of 7-inches thick, and total street and sidewalk width must be 20-feet wide and unobstructed.</p>
Standard Concrete Sidewalk	<p>Design as per the City's Design & Construction Standards Manual.</p> <p>*Note: To accommodate Fire Department ladder trucks, where pavement widths are less than 20-feet, then adjacent sidewalks must be a minimum of 7-inches thick, and total street and sidewalk width must be 20-feet wide and unobstructed.</p>

Curbs & Curb Ramps

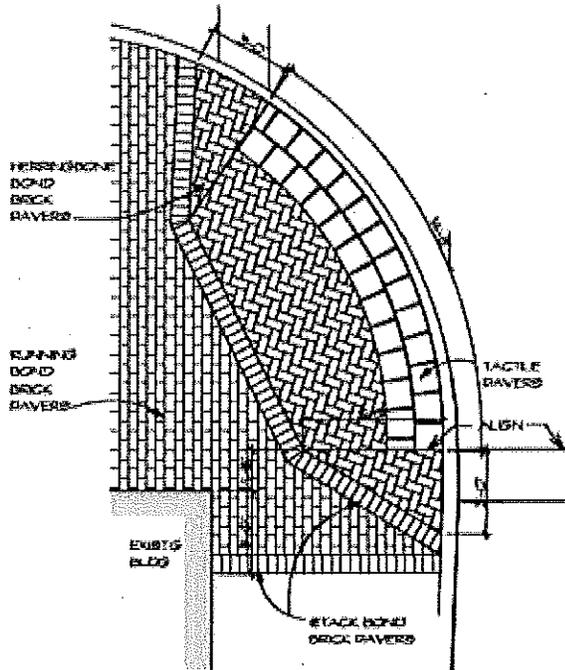
Curb

Pebble dyed concrete from Superior Concrete (540) 434-0346 or equivalent from other supplier. Other suppliers may name this shade differently and the tint may be different depending on the supplier.

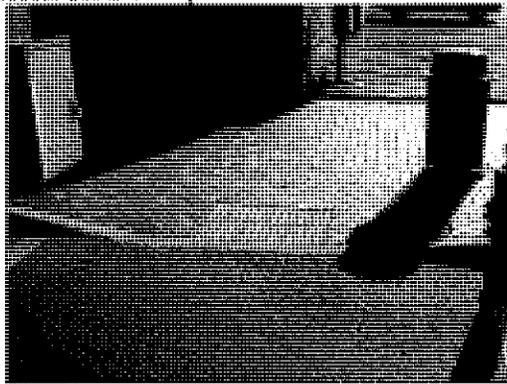
Brick Corner Curb Ramp



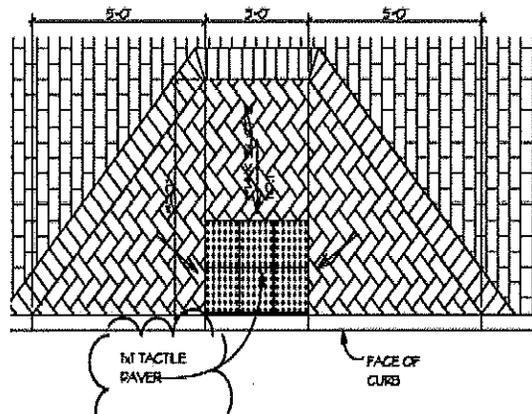
To be utilized where Brick Sidewalk is used.

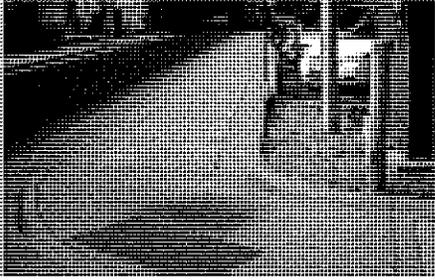


Brick Curb Ramp



To be utilized where Brick Sidewalk is utilized.



<p>Brick Tactile Pavers</p> 	<p>To be utilized where Brick Sidewalk is utilized. Hanover Detectable Warning Paver 11 3/4" X 11 3/4", Red 15.</p>
<p>Decorative Corner Curb Ramp</p> 	<p>Design as per the City's Design & Construction Standards Manual and VDOT Road & Bridge Standards with decorative concrete mix. As of 2009 edition, DCSM required CG-12A, B, or C as appropriate.</p>
<p>Decorative Typical Curb Ramp</p>	<p>Design as per the City's Design & Construction Standards Manual and VDOT Road & Bridge Standards with decorative concrete mix. As of 2009 edition, DCSM required CG-12A, B, or C as appropriate.</p>
<p>Decorative Tactile Pavers</p> 	<p>ADA Solutions (800) 372-0519 model 24 RAD REP (radial) or ID PAV 2 (24 X 48 insert) or equivalent.</p>
<p>Standard Corner Curb Ramp</p>	<p>Design as per the City's Design & Construction Standards Manual and VDOT Road & Bridge Standards with standard concrete mix. As of 2009 edition, DCSM required CG-12A, B, or C as appropriate.</p>
<p>Standard Typical Curb Ramp</p>	<p>Design as per the City's Design & Construction Standards Manual and VDOT Road & Bridge Standards with standard concrete mix. As of 2009 edition, DCSM required CG-12A, B, or C as appropriate.</p>
<p>Standard Tactile Pavers</p>	<p>ADA Solutions (800) 372-0519 model 24 RAD REP (radial) or ID PAV 2 (24 X 48 insert) or equivalent.</p>

Crosswalks

Decorative Crosswalk



TrafficPatternsXD (stamped asphalt, previously called StreetPrintXD) or equivalent from other supplier. – Pattern: diagonal herringbone. Color: Colonial Brick. Border: 12-inch wide white thermoplastic.

Standard Crosswalk

To be utilized where Decorative Crosswalk is not used and shall be a “Continental-Style” thermoplastic crosswalk.

Federal Street Path

Surface shall be red-dyed concrete.

Utilize the Manual on Uniform Traffic Control Devices (MUTCD) and Virginia Supplement to the MUTCD.

*Note: Where adjacent to roadway and to accommodate Fire Department ladder trucks, where pavement widths are less than 20-feet, then adjacent sidewalks must be a minimum of 7-inches thick, and total street and sidewalk width must be 20-feet wide and unobstructed.

Bicycle Facilities

Utilize the Manual on Uniform Traffic Control Devices (MUTCD) and Virginia Supplement to the MUTCD.

Street Paving

Standard asphalt to be used on all streets.

Street Lighting



To be installed within the Downtown Core, Transition Area, and along Gateways.

Holophane Memphis Style Luminare, Boston Harbour, Columbia 20, with banner bracket where appropriate. Color: Bronze. Include weatherproof receptacle at midpoint between hanner brackets and photo eye on each pole.

Traffic Signals

Decorative Traffic Signal

See Streetscape Standards map for locations.

Design is dependent upon location. Generally, bronze powder coated poles, arms, and signal heads (without back plates).

See City's "Specifications & Guidelines for Traffic Signal Projects" for more details.

Standard Traffic Signal

To be installed at signalized locations where the Decorative Traffic Signal is not used.

Galvanized steel mast arms and poles. See City's "Specifications & Guidelines for Traffic Signal Projects" for more details.

Bus Shelters/ Stops

Locations and design to be coordinated with Harrisonburg Department of Public Transportation.

Generally requires a minimum shelter pad of 9' X 15' with additional bus stop pad 8' deep for loading and unloading area.

Bicycle Racks



Inverted U steel tubing bike rack. May be connected in series or mounted as a single bike rack. Black powder-coat. May be in ground or surface mounted. Installation shall follow the "Bicycle Parking Guidelines: A set of recommendations from the Association of Pedestrian and Bicycle Professionals"

Signage

Blue Downtown Badge Street Name Sign



To be installed within Downtown Core and Transition Area, provided that all four sides of the intersection are fully within these areas.

At signalized intersections, this design shall be used in an LED street name sign with bronze housing. For additional LED street sign specifications, see City's "Specifications & Guidelines for Traffic Signal Projects".

LED Green Street Name Sign



To be installed at traffic signals along Gateways.

See City's "Specifications & Guidelines for Traffic Signal Projects".

Standard Green Street Name Sign

To be installed along non-signalized intersections along Gateways and fringes of the Transition Area.

Decorative Regulatory Street Sign Post



Generally, black powder-coated 2 in. x 2 in. square channel steel post without holes. Post topped with silver pyramidal cap (2x2 cast aluminum rain cap. Korman Item # HCC22 or equivalent).

Standard Regulatory Street Sign Post

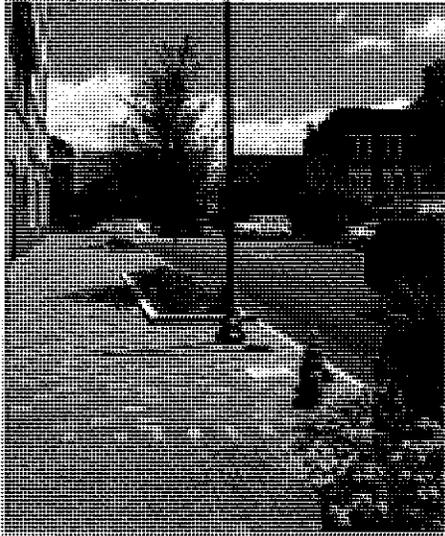
Generally, galvanized square channel steel post with holes.

Wayfinding Sign Program



Design dependent upon location. Generally, NW Sign Industries, Trailblazer "B" style sign. Contact City of Harrisonburg Public Works Department, (540) 434-5928, for specifications.

Landscaping

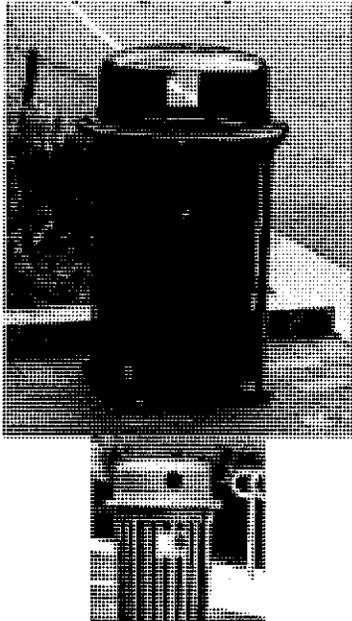


See Appendix C for further description of landscaping opportunities for downtown.

Use of in ground planters and free standing planter will be determined on a site by site basis.

At the time of this writing, free standing planters purchased for use on Main Street and around Court Square have been the Cast Stone Galveston Planter made by Dura Art Stone. Various shapes and sizes have been utilized.

Trash & Recycling Receptacles



Trash and recycling receptacles to be utilized in Downtown Core and Transition Area.

Scarborough Litter Receptacle. Style: side opening, 30 gallon capacity. Side panel: vertical strap. Color: Stormcloud Powdercoat. Liner: Default color, 30 gallon polyethylene liner. Standard features: free standing/ surface mountable.

Note: Image shown of sample recycling receptacle in silver color. Color will be: Stormcloud Powdercoat.

Benches

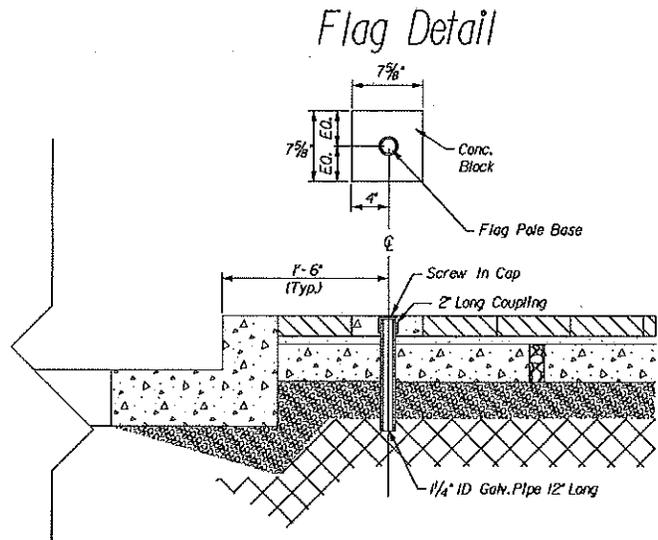


To be utilized in Downtown Core and Transition Area.

Landscape Forms Scarborough, backed, 72" length, horizontal strap seat with center arm (not pictured), Stormcloud Powdercoat Finish. (Other lengths available)

Flag Supports

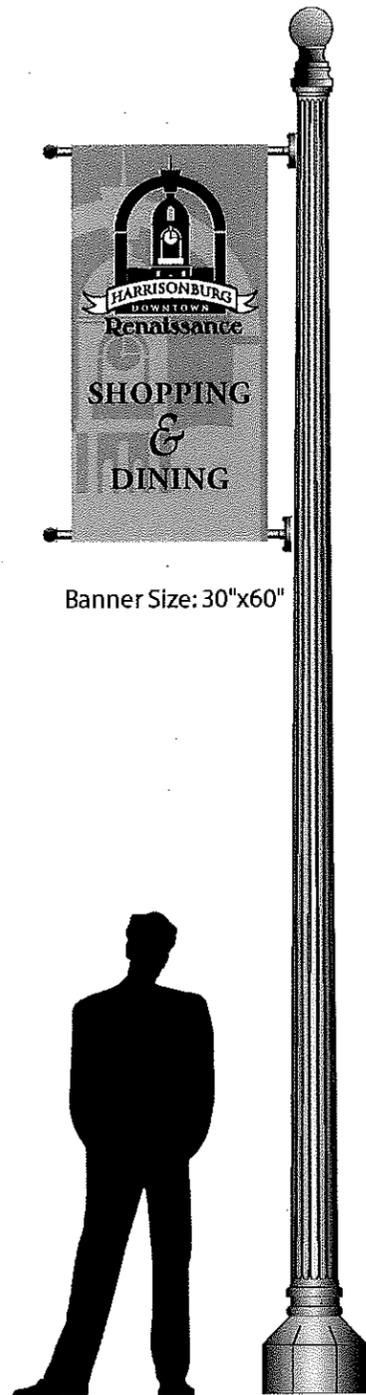
Flag supports are provided for installing flags along Main Street for special events. The detail below shall be used for their installation, with the following design exception: the Stream Walk Medallion should be excluded and the concrete block should be brought in on the right side of this detail.



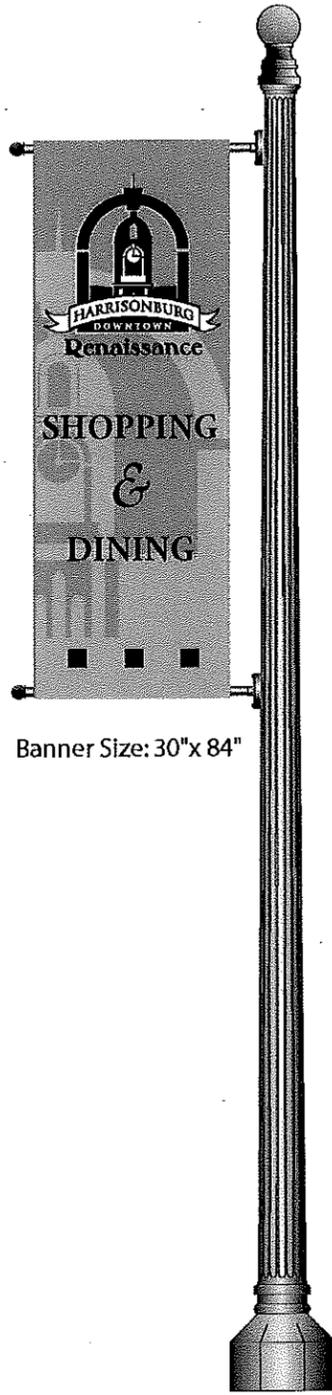
Banners



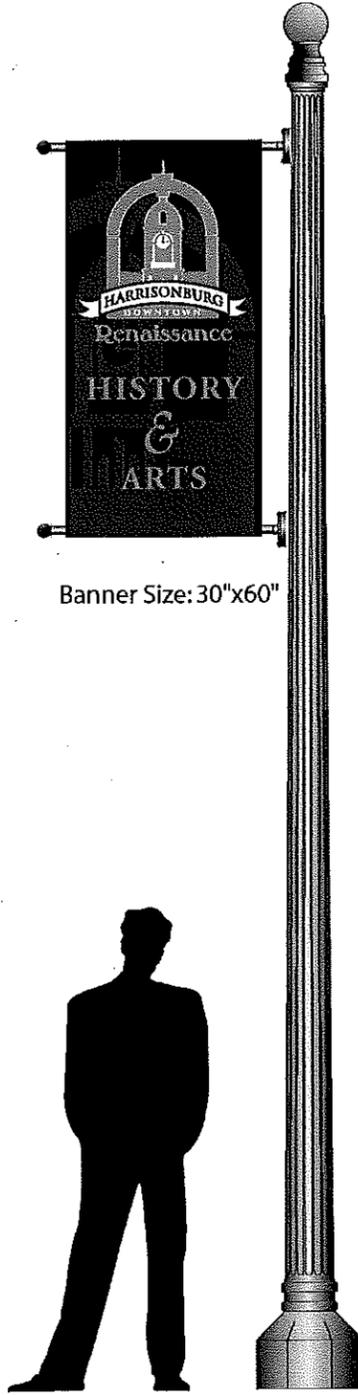
Banners are to be installed on all light poles on Main Street, Market Street, and Court Square within the Downtown Core and Transition Area. Banner design should match the specifications on the following page. Consult with the Public Works Department prior to installation for details on banner selection and location. If desired by the City and/or Harrisonburg Downtown Renaissance, new banner designs may be taken into consideration by those parties.



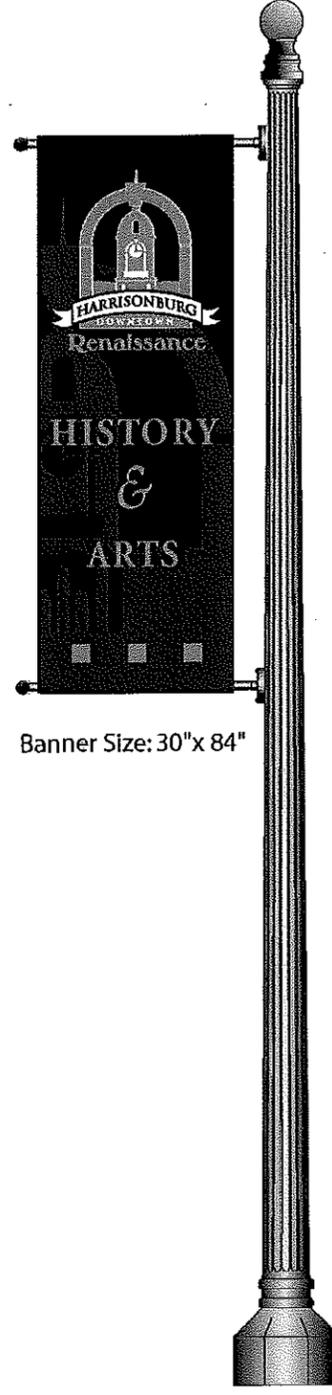
Banner Size: 30"x60"



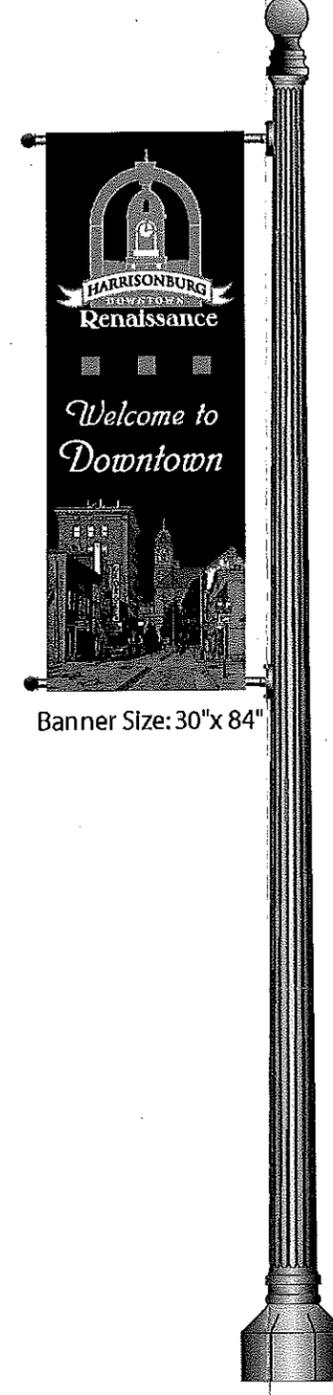
Banner Size: 30"x 84"



Banner Size: 30"x60"



Banner Size: 30"x 84"



Banner Size: 30"x 84"



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ARCHITECTURE & PLANNING
213 NORTH AUGUSTA STREET, STAUNTON, VA 24401
PHONE 540.886.6230 FAX 540.886.0629
www.frazierassociates.com

Harrisonburg Downtown Schematic Sign Design
Harrisonburg, Virginia

Job Number: 2003.104
Date: February 8, 2005
Sheet 1 of 1

APPENDIX C: LANDSCAPING

Trees and landscaping are highly visible elements within the streetscape. They provide spring bloom, summer shade, fall foliage color, and winter branching. Landscaping softens the otherwise hard urban environment, provides a defense against the “urban heat island effect” and can help with stormwater management.

At the time of this writing, maintenance of trees and in ground planters within the downtown streetscape is being performed by the City Department of Parks & Recreation. However, maintenance is minimal and opportunities to add more plantings are not available due to limitations on budgets and available space. Maintenance of free standing planter boxes and containers along the streetscape has been organized by Harrisonburg Downtown Renaissance and is being provided, in part, by businesses downtown that have volunteered to hand water and maintain the planters.

Maintenance must be considered in the choice of plant materials, and maintenance concerns must be addressed early in the streetscape and landscaping design process, along with property access and access for emergency responders. Although no landscaping will be successful without some degree of maintenance, some plants require less attention than others. Landscaping, both in ground planters and free standing planters, will be determined on a site by site basis. Some considerations may include width of sidewalk, whether there is on street parking adjacent to the area, availability of light onto the site, etc.

Lack of proper growing space is one of the primary causes of premature death of urban trees. Where sidewalks are less than 9' wide, street trees are not recommended. In these situations, plantings may be added to the streetscape by alternative means such as:

- Hanging baskets on light poles (this feature may require additional engineering review to determine the suitability of installation on the standard light pole called for in the Design Standards & Descriptions section).
- Private planter boxes along buildings or hanging from adjacent buildings.



Example of privately maintained planter boxes at Hawthorne Hotel in Salem, MA

- Side street curb extensions at intersections with large planters.
- Free standing planter boxes outside of the pedestrian pathway.

Where more sidewalk space is available, but space is still limited (sidewalks 9'-12' wide), trees in tree grates, 4'x6' or 5'x5' in size, or the use of silva cells may be considered.

It is recommended that different variety of trees should be used throughout downtown. However, each street or section of street should have a consistent look with the same variety of trees spaced along the street.

APPENDIX D: SHARED USE PATHS AND PEDESTRIAN PATHS

This Plan seeks to formalize the alignments for two shared use paths and walkway facilities in the downtown area. These facilities will connect with the larger development of the Northend Greenway, the Bluestone Trail, and trail facilities outside of the downtown area that are included in the City's 2010 Bicycle & Pedestrian Plan (amended 2011).

It should be noted that the following concepts for the Federal Street shared use path and Pedestrian Path (Municipal Lot to Liberty Park) were not considered during the development of the 2010 Bicycle & Pedestrian Plan. Should City Council approve these concepts, it is intended that the Bicycle & Pedestrian Plan will be updated to reflect these additions.

FEDERAL STREET SHARED USE PATH

Federal Street is a quiet street in the heart of the downtown core. This low volume street is a narrow alleyway south of East Water Street and a two-lane street north of East Water Street. It is primarily used for deliveries, making short trips between businesses and parking lots and serving as the ingress/egress point for the top of the Elizabeth Street parking deck. Its low traffic and potential for conversion to one-way motor vehicle traffic (flowing northbound north of East Market Street and southbound south of East Market Street), makes this street an excellent location for creating a recreational path in parallel with the street, including attractive landscaping and public art.

The following renderings show a shared use path corridor, generally 10-feet wide for bicyclists and pedestrians, stretching between the Turner Pavilion to the southern terminus of the Northend Greenway. The heart of the corridor will be Federal Street between East Wolfe Street to Franklin Street.

Due to unknown development specifics and challenges with existing conditions, three alignment options are provided north of East Wolfe Street:

- Option A is the preferred alignment as it is a straight continuation of Federal Street, but Option A has challenges such as going through an area behind the Friendly City Food Co-op, Family Dollar, and the U.S. Post Office that is heavily utilized for deliveries and refuse service, and in close proximity to parking spaces adjacent to Kline's Dairy Bar. Between East Elizabeth Street and East Gay Street, the Colonnade at Rocktown had dedicated half the width needed for a pedestrian easement to the City of Harrisonburg, but additional easements from adjacent property owners are still needed.
- Options B and C are alternative alignments that may be considered depending upon how the area redevelops and the willingness of private property owners to dedicate land for this path.

From where Federal Street meets Franklin Street, two options are shown for connecting to the Turner Pavilion:

- Option D would utilize Federal Street between Franklin Street and Campbell Street, which is a narrow 19-foot wide alley that serves adjacent parking lots. Like the block between Bruce Street to Franklin Street, bicyclists, pedestrians, and motorists would have to share this space, as there is not enough room to provide separation between bicyclists and pedestrians from motor vehicles. Option D also requires the relocation or burying of utility

lines on the north side of Campbell Street in order to accommodate a 10-foot wide shared use path and buffer area. This would require that Campbell Street serve only one lane of one-way traffic. Crossing South Main Street would become easier as the proposed bump outs at this intersection will reduce the crossing distance.

- Option E would have a shared use path constructed on the north side of Franklin Street, and the path would cross South Main Street. Along South Main Street, the sidewalk in front of the Virginia Quilt Museum would be widened, and the path is proposed to turn into the drive way between the Quilt Museum and the Smith House, and connect with the shared use path/pedestrian walkway that will lead to the Turner Pavilion.
- Interconnections between the shared use path and adjacent points of interest could be considered where public property is available and where private property owners request to provide direct customer access to businesses from the path. For example, it would be beneficial to create a convenient connection between Federal Street and Court Square.



South Federal Street before improvements (looking north from East Water Street)



South Federal Street after improvements (looking north from East Water Street)

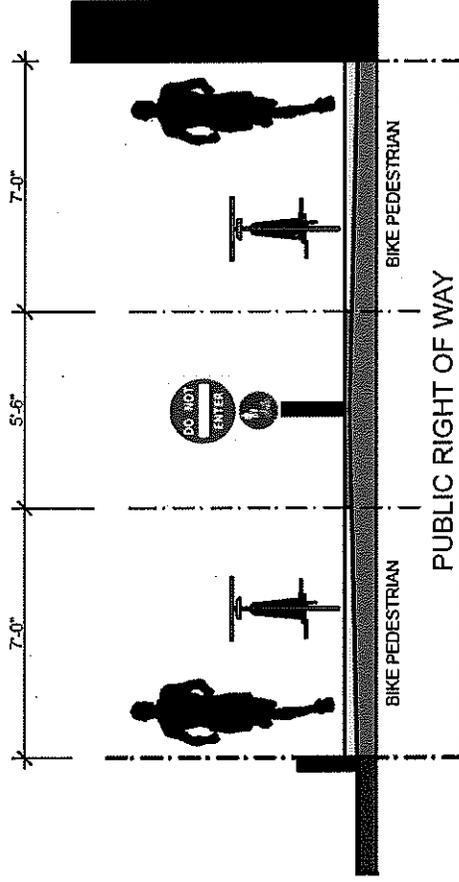
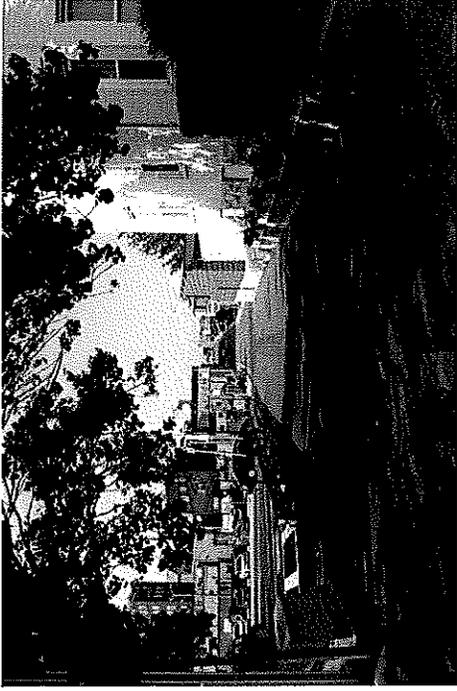
DOWNTOWN FEDERAL STREET CORRIDOR

PROPERTY RECORDS - GIS FILE DEVELOPMENT

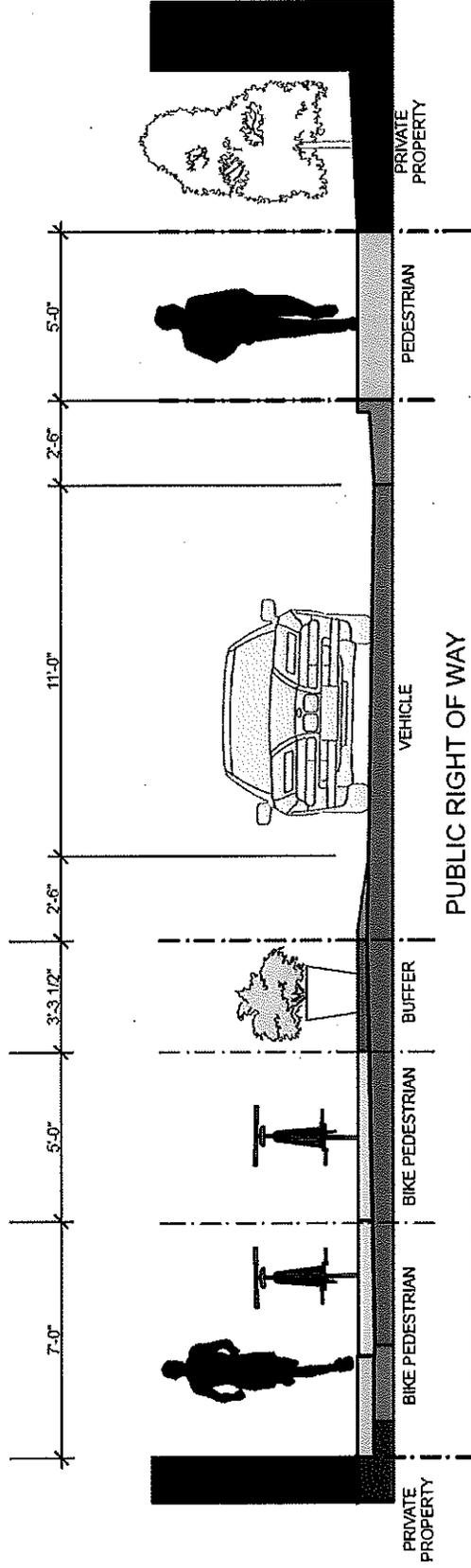
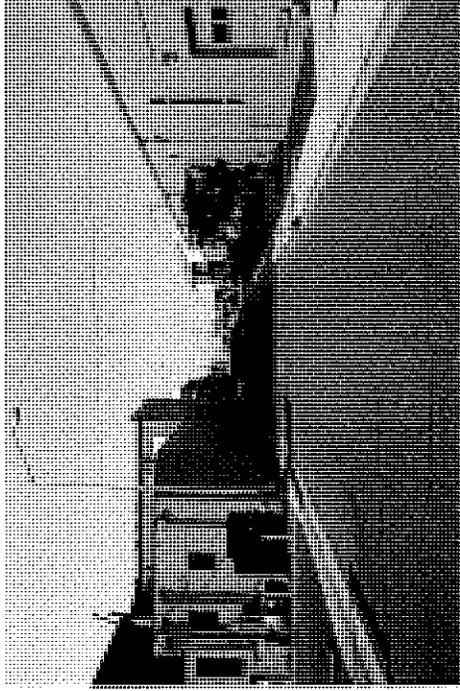
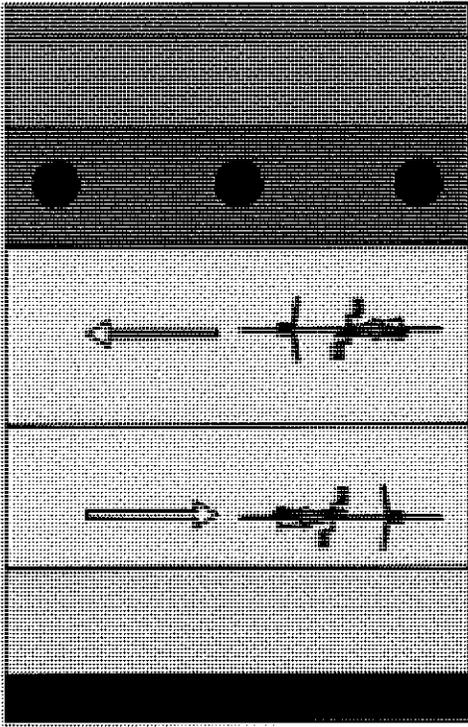


FARMERS MARKET

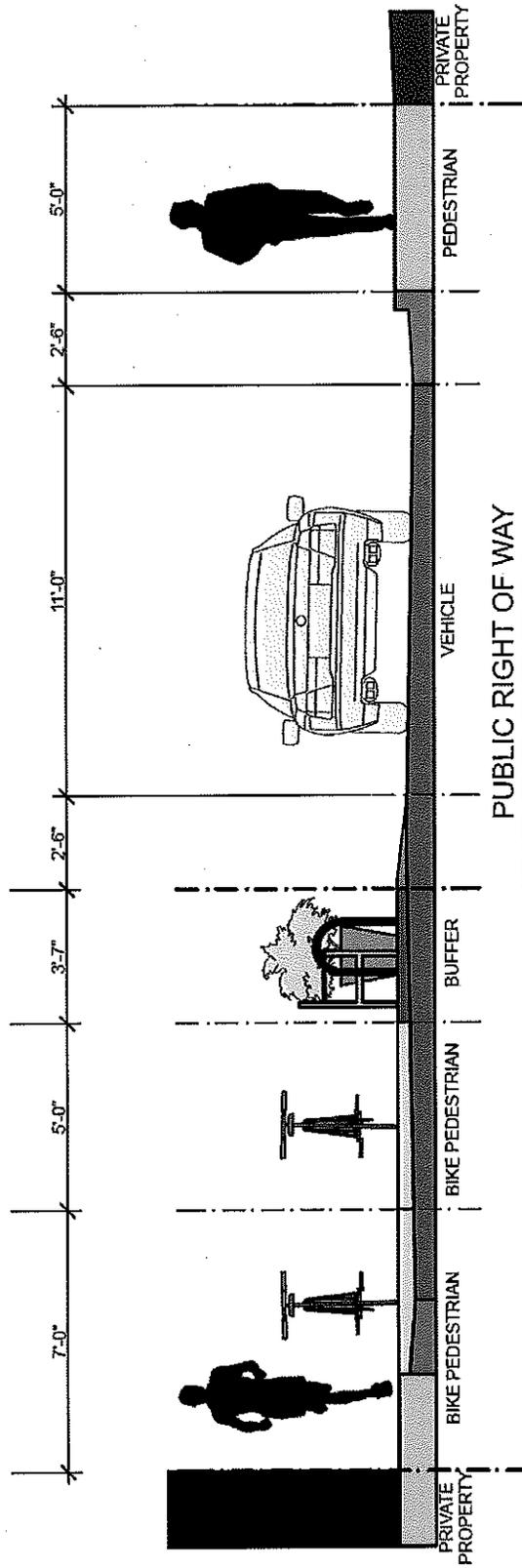
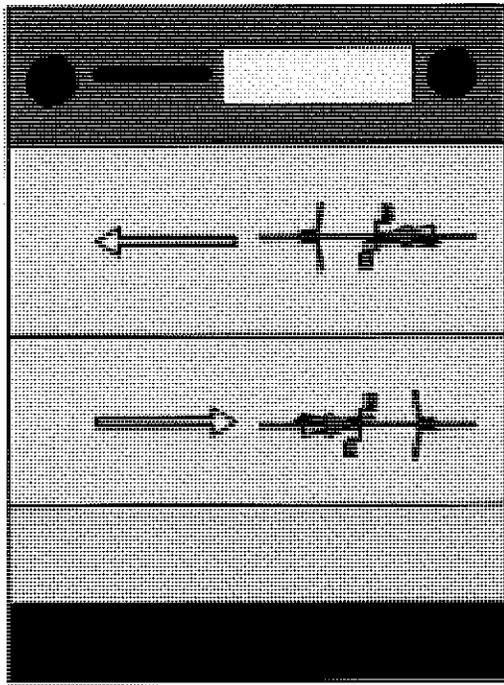
1. Newman Avenue to East Water Street



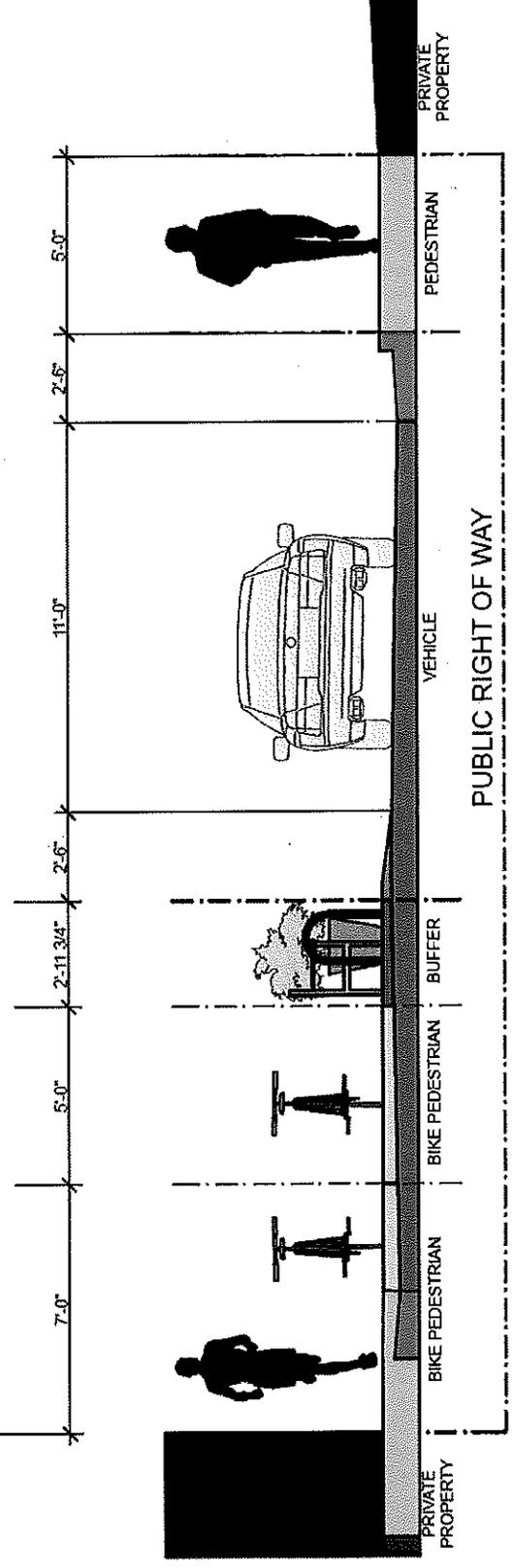
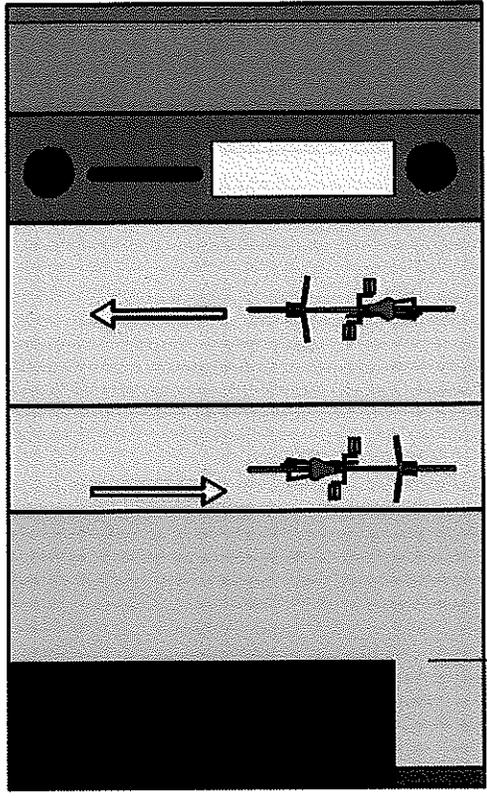
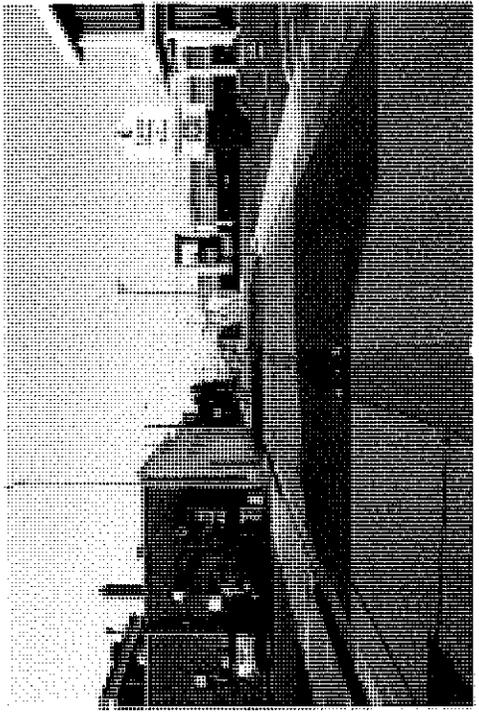
3. East Water Street to East Market Street (Northern Segment)



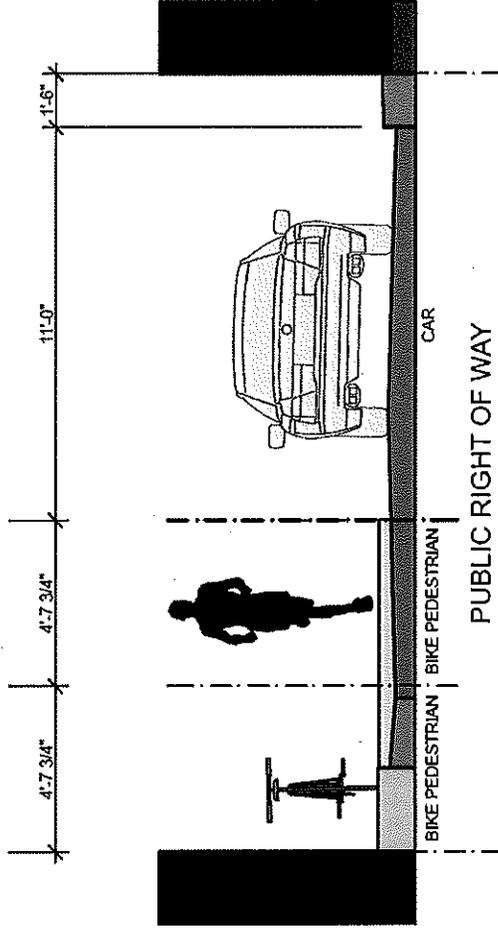
4. East Market Street to East Elizabeth Street (Southern Segment)



5. East Market Street to East Elizabeth Street (Northern Segment)



7. Blessed Sacramento Catholic Church to East Wolfe Street



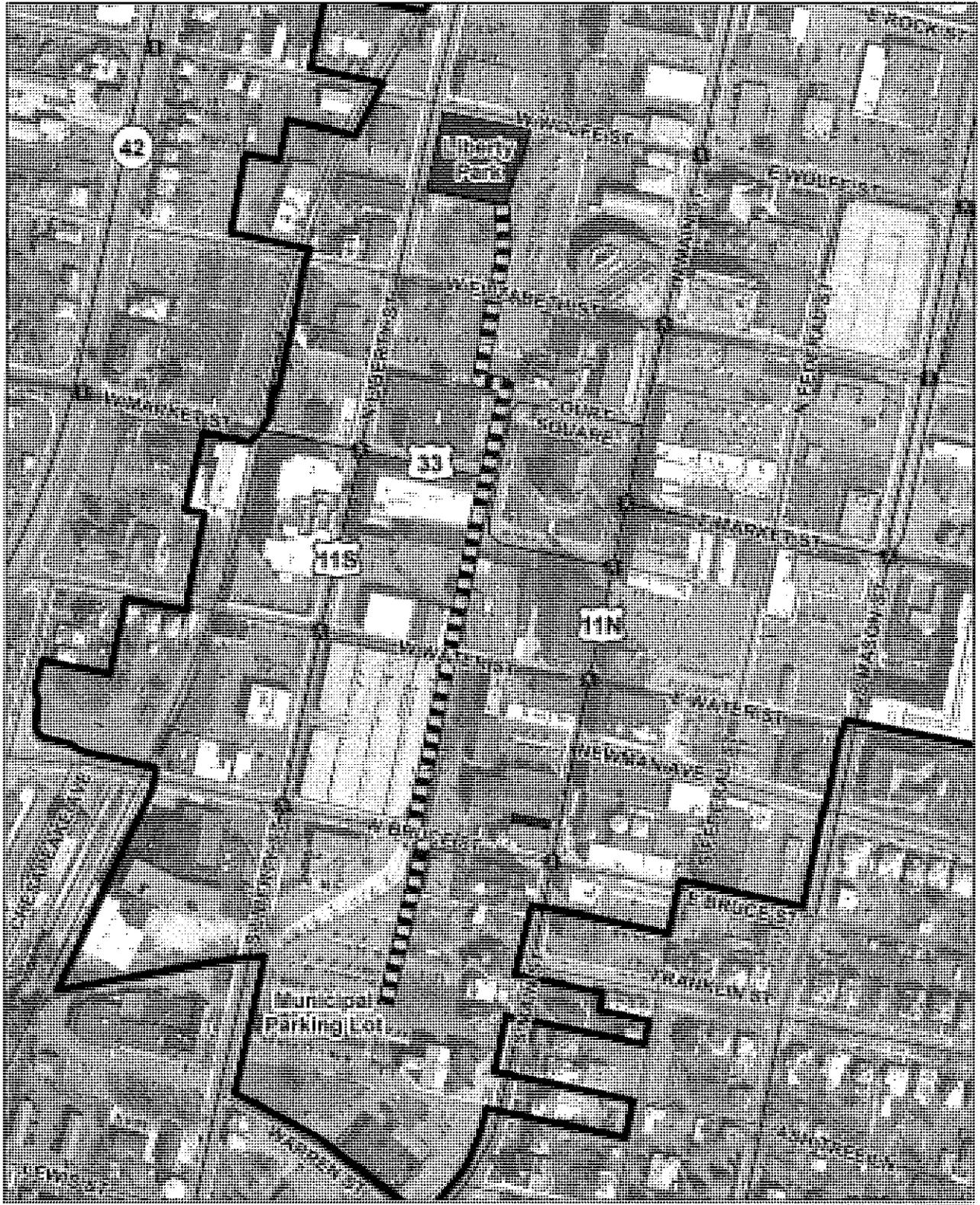
PEDESTRIAN PATH (MUNICIPAL LOT TO LIBERTY PARK)

It is recommended that the City consider enhancing the pedestrian path between the municipal parking lot to Court Square to Liberty Park with benches, trees and green space.

Between the Municipal Parking Lot and Court Square, this path would be a 10- to 20- foot wide corridor. This concept would redevelop an existing corridor from a vague pathway that is generally obscure to Harrisonburg's visitors and plain to its residents into a pedestrian pathway of greater vitality, connecting what are becoming the two cores of downtown social and economic development, Turner Pavilion and Court Square. Central to the theme of this proposal is the historic nature of the City. The proposed path travels through the heart of the original boundaries of Harrisonburg and passes adjacent to the stone house of the founder of the City, Thomas Harrison, whose original land grant of the early 18th century incorporates the lands contained within the course of the walkway.

Two of the more technically challenging aspects of this pathway's design is its integration with the Municipal Parking Lot and the existing bridge over Blacks Run on West Bruce Street, which obscures vehicle sight lines. To help mitigate these issues, it is recommended that the concept of closing the West Bruce Street exit from the municipal lot be explored as a means of limiting pedestrian conflicts with motor vehicles.

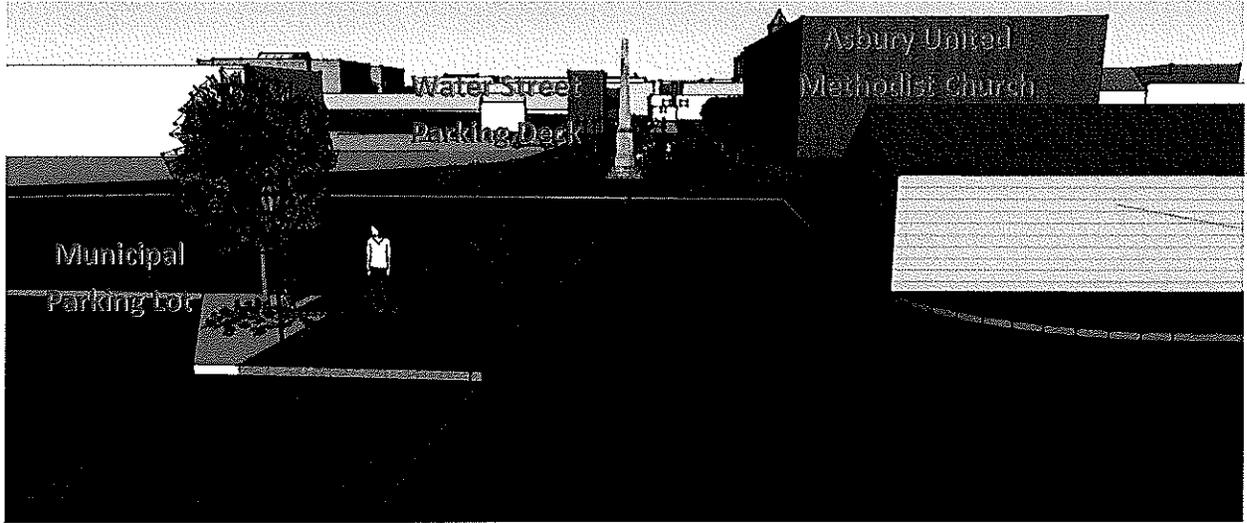
This route would also provide an improved pedestrian route for those using the Water Street parking deck. From Court Square, the path would travel north towards Liberty Park along the alignment shown in the Pedestrian Path map.



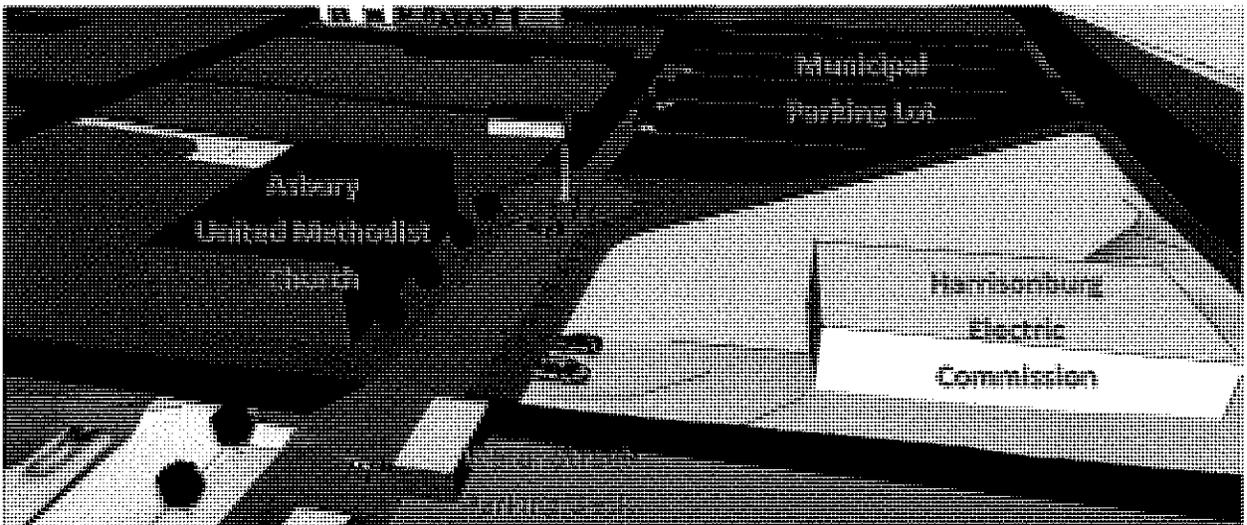
Pedestrian Path
 Street
 Transitway
 Boundary
 Parking Lot

Pedestrian Path
Downtown Streetscape Plan





Pedestrian Path (concept for connection from Municipal Parking Lot to Water Street Parking Deck)

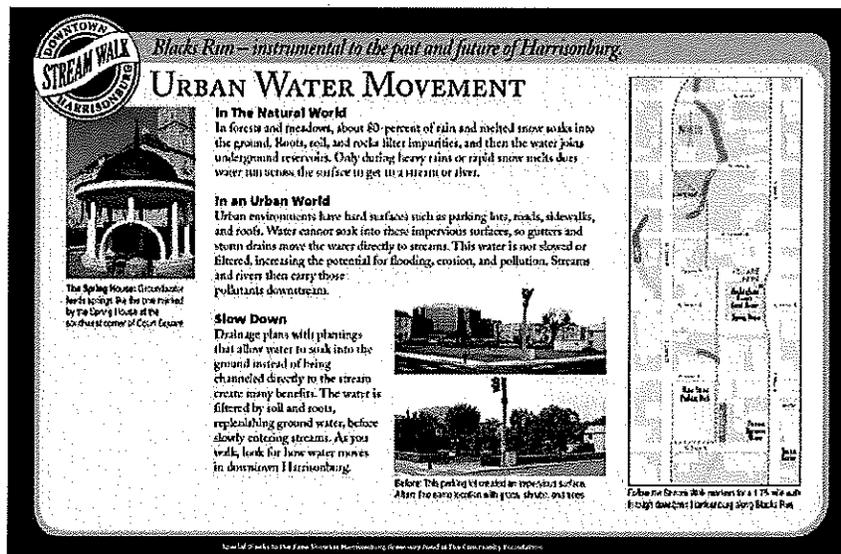


Pedestrian Path (concept for connection from Municipal Parking Lot to Water Street Parking Deck)

DOWNTOWN STREAM WALK

In 2008, the Friends of Blacks Run Greenway, with financial support from The Community Foundation, received approval from City Council to delineate a 1.75 mile Downtown Stream Walk that followed Main Street and Liberty Street between the World War II Memorial to the south and the intersection of North Main Street and Noll Drive to the north. The walk's objective was to create a pedestrian path for exercise, encouraging healthy enjoyment of businesses, restaurants, pocket parks, and green spaces in downtown.

The project included the installation of four educational, interpretive signs that highlighted Blacks Run and its history and role in the past and future of Harrisonburg. The signs are titled and reflect the topics of: "Growing on Blacks Run", "Blacks Run Wildlife", "Landscaping for Stream Health", and "Urban Water Movement". The walking route was delineated with 93, 3-inch diameter bronze medallions embedded into existing sidewalks along the walking route. However, over the years, the medallions have badly tarnished, making them difficult to see in the sidewalks, and some have also become loose from the sidewalk and have gone missing.



One of four signs installed for the Downtown Stream Walk

Although there is desire to support the concept and vision of the Downtown Stream Walk, at the time of this writing, city staff has decided not to reinstall the Downtown Stream Walk medallions until a new medallion design is developed (that are more visible and better affixed into the sidewalk), and until pathways are delineated as part of this plan. It would be a positive opportunity to combine the objectives of the Downtown Stream Walk with the Pedestrian Path between the Municipal Lot and Liberty Street, and the Federal Street Shared Use Path.

May 2014 Proactive-Zoning Report

For the month of May 2014 the proactive-zoning program inspected the **Avalon Woods** section of the city. During the proactive inspections thirty-six violations were found. The violations consisted of inoperable vehicles, junk, and tall, grass and weeds.

MONTH	SECTOR	4 th CYCLE VIOLATIONS	CORRECTED	1 st CYCLE	2 nd CYCLE	3 rd CYCLE
December 2011	Wyndham Woods	2	2	2	0	4
January 2012	Northfield	13	13	21	6	19
February 2012	Purcell Park	8	8	7	6	5
March 2012	Parkview	5	5	19	7	16
April 2012	Ind./Tech Park	0	0	0	1	0
May 2012	Northeast	29	29	80	45	63
June 2012	Exit 243	1	1	10	0	1
July 2012	Fairway Hills	2	2	1	0	0
August 2012	Smithland Rd.	2	2	0	4	0
September 2012	N. Main St.	10	10	13	4	4
October 2012	Liberty St.	11	11	6	4	18
November 2012	Westover	13	13	18	8	17
December 2012	Garbers Church	9	9	1	2	1
January 2013	Spotswood Acres	8	8	6	4	1
February 2013	Jefferson St.	21	21	26	22	35
March 2013	Forest Hills/JMU	1	1	6	1	1
April 2013	S. Main St.	5	5	1	0	2
May 2013	Hillandale	11	11	7	5	17
June 2013	Maplehurst/JMU	0	0	6	5	2
July 2013	Long Ave/Norwood	11	11	12	28	17
August 2013	Greystone	9	9	13	10	13
September 2013	Greendale/SE	1	1	3	2	5
October 2013	Ramblewood	11	11	4	8	1
November 2013	Stone Spring Village/JMU	2	2	2	10	0
December 2013	Sunset Heights	2	2	7	29	10
January 2014	Reherd Acres	10	10	10	12	9
February 2014	RT 33 West	13	13	0	16	6
March 2014	Chicago Ave	4	4	16	22	29
April 2014	Pleasant Hill	9	7	4	13	17
May 2014	Avalon Woods	36	n/a	7	26	11
June 2014	Waterman Elementary			6	61	18
July 2014	Keister Elem			6	5	8
August 2014	500-600 S. Main			7	30	16
September 2014	Court Square			0	3	2
October 2014	Bluestone Hills & Valley Mall			3	33	31
November 2014	Preston Heights			8	3	1

The proactive-zoning program for June 2014 will be directed towards the enforcement of the Zoning Ordinance in the **Waterman Elementary** section of the City.